

CITY OF PORTLAND PUBLIC WORKS DEPARTMENT

CONTRACT DRAWINGS

2025 SIDEWALK AND ADA PROJECT BID NUMBER:

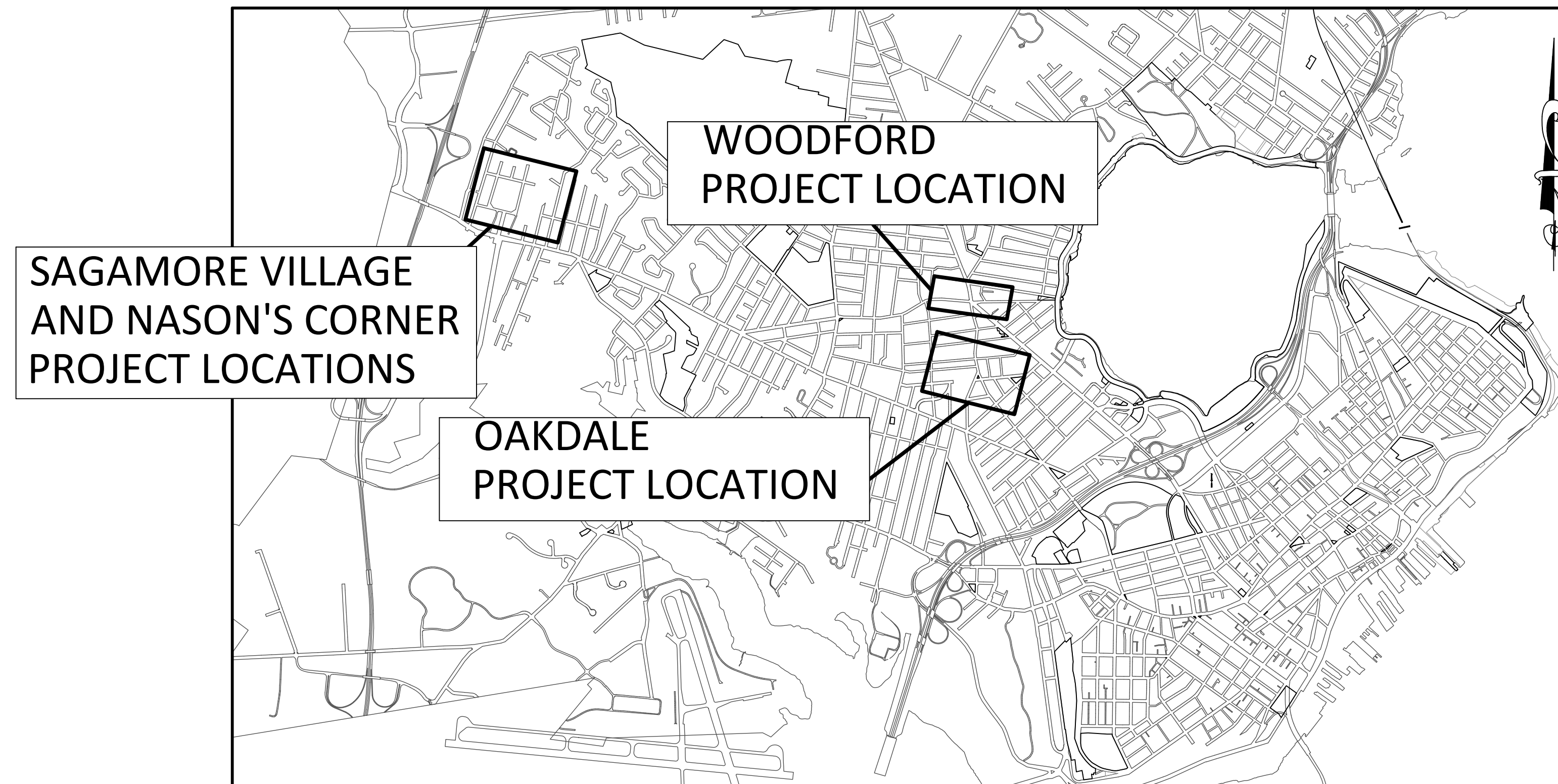
PROJECT LOCATIONS

- SAGAMORE VILLAGE: POPHAM STREET AND PURCHAS STREET
- WOODFORD STREET: NEVENS STREET TO FOREST AVENUE
- NASON'S CORNER: PURCHAS STREET AND WARWICK STREET SIDEWALKS
- OAKDALE PROJECT: LONGFELLOW STREET FROM DEVONSHIRE TO OAKDALE STREET

YEAR
APPROVED
2024

CONSTRUCTION PLANS

xxxxxx 2024



KEITH GRAY, P.E.
CITY ENGINEER DATE

EMILY BOLT, P.E.
PROJECT MANAGER DATE

INDEX OF PLANS

NO.	TITLE
0	COVER SHEET
1	GENERAL NOTES
2-9	EXISTING CONDITION PLANS
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19-22	TYPICAL DETAILS

GENERAL NOTES:

- PROTECT EXISTING FEATURES/STRUCTURES NOT CALLED OUT FOR REPLACEMENT/ALTERATION.
- THE CITY OF PORTLAND SHALL HAVE THE RIGHT AND AUTHORITY TO DETERMINE THE ACCEPTABILITY OF WORK AND MATERIALS IN PROGRESS OR COMPLETED. THE CITY OF PORTLAND SHALL HAVE THE RIGHT TO REJECT ANY WORK OR MATERIALS WHICH DO NOT CONFORM, IN ITS SOLE OPINION, TO THE PLANS OR SPECIFICATIONS.
- PRIOR TO THE BEGINNING OF CONSTRUCTION, THE CONTRACTOR SHALL SECURE A STREET OPENING PERMIT FROM THE PORTLAND DEPARTMENT OF PUBLIC WORKS. NO FEE WILL BE CHARGED FOR THIS PERMIT.
- ALL MATERIAL SCHEDULES SHOWN ON THE PLANS ARE FOR GENERAL INFORMATION ONLY. THE CONTRACTOR SHALL PREPARE THEIR OWN MATERIAL SCHEDULES BASED UPON THEIR PLAN REVIEW. ALL SCHEDULES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS OR PERFORMING WORK.
- PRIOR TO ORDERING CURB, THE CONTRACTOR SHALL REVIEW LOCATIONS OF EXISTING CURB JOINTS IN FIELD AND CONFIRM LENGTHS OF NEW CURBING ADJACENT TO EXISTING CURBING.
- CONTRACTOR SHALL USE CAUTION WHEN WORKING NEAR EXISTING OVERHEAD LINES.
- DISPOSITION OF SURPLUS MATERIAL SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. SURPLUS MATERIAL SHALL NOT BE DISPOSED OF ON THE PROJECT SITE. DISPOSITION SHALL BE MADE ONLY AT WASTE AREAS WHICH ARE LICENSED TO ACCEPT SUCH MATERIALS, UNLESS THE MATERIALS CAN BE INCORPORATED IN FILLS IN OTHER PROJECTS OF THE CONTRACTOR.
- IF CONTRACTOR PROPOSES TO TEMPORARILY STOCKPILE ANY SURPLUS SOIL AND ROCK IN THE CITY OF PORTLAND, THE CONTRACTOR SHALL OBTAIN APPROVAL FOR EACH STOCKPILE LOCATION FROM THE CITY. IF CONTRACTOR PROPOSES TO PERMANENTLY STOCKPILE ANY SURPLUS SOIL AND ROCK ON PROPERTY IN THE CITY OF PORTLAND, THE CONTRACTOR MUST OBTAIN ANY SITE PLAN AND FILL PERMITS REQUIRED FROM CITY PLANNING AUTHORITY OR ANY FILL PERMITS REQUIRED FROM MAINE DEP OR US ARMY CORP OF ENGINEERS. BOTH TEMPORARY AND PERMANENT STOCKPILE LOCATIONS SHALL MEET THE APPLICABLE SETBACK REQUIREMENTS IN THE CITY LAND USE CODE.
- MAINTENANCE OF TRAFFIC SHALL BE FOLLOWING THE MOST CURRENT VERSION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND THE MAINE DOT SPECIFICATIONS AND STANDARD PLANS.
- THE CONTRACTOR SHALL CALL THE APPROPRIATE UTILITY COMPANY AND DIG SAFE (888-344-7233) AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR THE ELEVATION OF THE EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND WHERE POSSIBLE MEASUREMENTS TAKEN IN THE FIELD. THE FOLLOWING UTILITIES MAY HAVE FACILITIES WITHIN THE PROJECT LIMITS: - CENTRAL MAINE POWER COMPANY - UNITIL - PORTLAND FIRE DEPARTMENT - PORTLAND WATER DISTRICT - CITY OF PORTLAND - SPECTRUM - CONSOLIDATED COMMUNICATIONS - AT&T ANY ADDITIONAL UTILITY WORK NOT SPECIFIED ON THE PLANS SHALL BE COMPLETED BY THE RESPECTIVE UTILITY COMPANY. IT SHOULD BE NOTED THAT RECENT UPGRADES TO THE GAS LINE WERE MADE BY UNITIL/NEUCO IN AUGUST 2020. THIS WORK WAS CONDUCTED AFTER THE SURVEY WAS COMPLETED.
- CONTRACTOR SHALL COORDINATE ANY DISRUPTION OF PRIVATE UTILITY SERVICES WITH LAND OWNER AT LEAST 2 DAYS (48 HOURS) PRIOR TO SCHEDULED DISRUPTION.
- EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH OSHA REGULATIONS SUBPART P OF 29 CFR PART 1926.650-.652 (CONSTRUCTION STANDARD FOR EXCAVATIONS).
- LOCATIONS OF RIGHT-OF-WAY SHOWN ON PLANS ARE APPROXIMATE ONLY.
- THE CONTRACTOR SHALL COMPLETE THE WORK WITHIN THE RIGHT-OF-WAY, AND SHALL BE RESPONSIBLE IF TRESPASSING ON PRIVATE PROPERTY OCCURS.
- CONTRACTOR SHALL NOT PARK, IMPEDE ACCESS OR STORE EQUIPMENT/MATERIAL ON ADJACENT PRIVATELY OWNED LAND WITHOUT WRITTEN CONSENT FROM THE CITY OR LAND OWNER.
- THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED OR DAMAGED BY CONSTRUCTION ACTIVITIES, TO ORIGINAL FINISH SURFACE (LAWN, PAVEMENT, GRAVEL, ETC.) UNLESS NOTED OTHERWISE ON PLANS. RESTORATION OF PAVED SURFACES, GRAVEL SURFACES, DRIVEWAYS, WALKWAYS, LAWNS AND OTHER AREAS SHALL BE AT THE CONTRACTORS EXPENSE. ALL CURB DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE REPLACED IN KIND, AT THE CONTRACTORS EXPENSE, AND SHALL CONFORM TO CITY OF PORTLAND STANDARDS.
- ALL SIGNING, SIGNAL AND STRIPING MATERIALS AND PLACEMENT SHALL CONFORM TO THE MOST CURRENT EDITION OF THE MAINE DOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND STANDARD DETAILS AND WITH THE FEDERAL HIGHWAY ADMINISTRATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
- BUTT JOINTS SHALL BE USED AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT MEETS EXISTING PAVEMENT. NO FEATHERING OF PAVEMENT WILL BE PERMITTED. COST OF BUTT JOINTS SHALL BE INCIDENTAL TO PAY ITEMS IN SECTION 403 - HOT BITUMINOUS PAVEMENT AND SECTION 608 - SIDEWALKS AND DRIVEWAYS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY TRENCH PAVEMENT THAT HAS EXPERIENCED EXCESSIVE SETTLEMENT, CRACKING, OR OPENING OF JOINTS. REPAIRS MAY INCLUDE OVERLAY, REMOVAL OF UNACCEPTABLE MATERIALS, COMPLETE REPLACEMENT, JOINT SEALING, OR RECONSTRUCTING PAVEMENT JOINTS AS REQUIRED. THIS WORK MAY BE NECESSARY AFTER THE FINAL ACCEPTANCE OF WORK OR PRIOR TO THE ONE YEAR GUARANTEE. THIS WORK SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
- ALL WORK COMPLETED UNDER THIS CONTRACT SHALL BE GOVERNED BY AND SHALL CONFORM WITH CITY OF PORTLAND TECHNICAL AND DESIGN STANDARDS AND GUIDELINES.
- THE CONTRACTOR SHALL ANTICIPATE THAT GROUNDWATER WILL BE ENCOUNTERED DURING CONSTRUCTION AND SHALL INCLUDE SUFFICIENT COSTS WITHIN THEIR BID TO PROVIDE DEWATERING AS NECESSARY. NO SEPARATE PAYMENT SHALL BE MADE TO THE CONTRACTOR FOR DEWATERING.

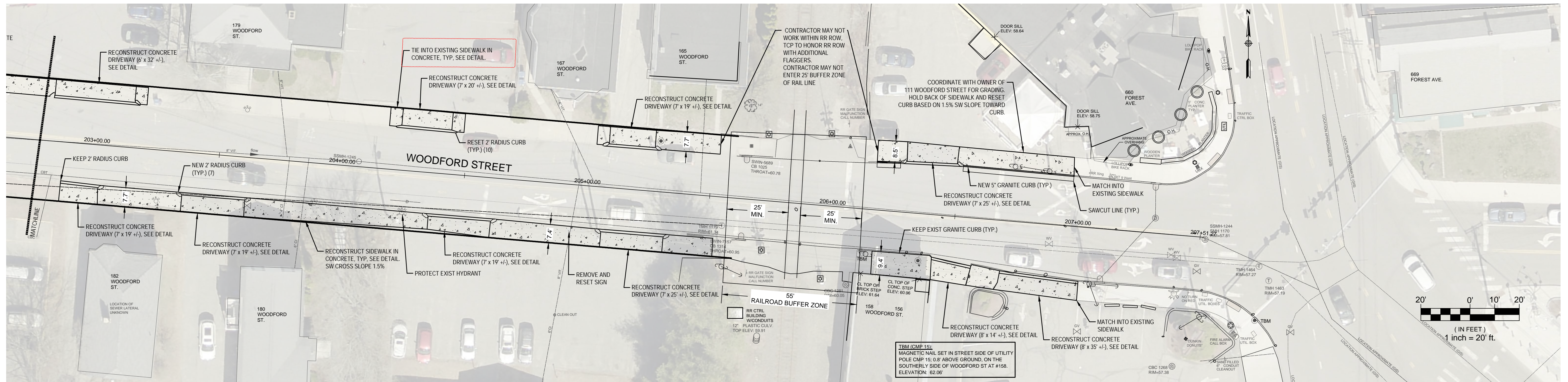
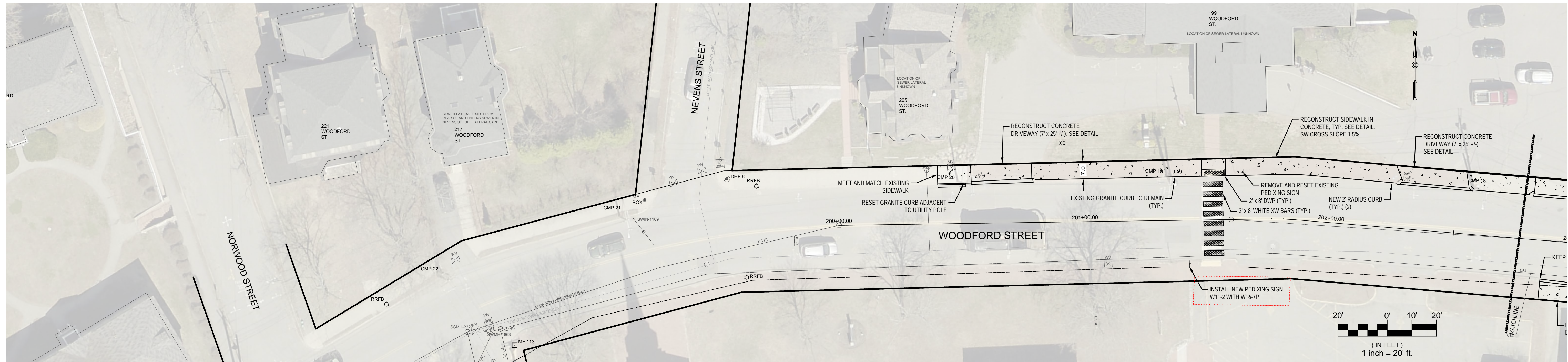
- EXISTING FACILITIES/STRUCTURES (I.E. TREES, POLES, LIGHT POLES) SHALL BE REMOVED AND PROTECTED DURING CONSTRUCTION. CITY RETAINS THE RIGHT TO KEEP ANY AND ALL REMOVED FACILITIES/STRUCTURES. CONTRACTOR SHALL DISPOSE OF UNWANTED/UNUSED FACILITIES/STRUCTURES OFF SITE IN CONFORMANCE WITH APPLICABLE FEDERAL, STATE, AND LOCATION REGULATIONS.
- ALL MATERIALS AND CONSTRUCTION METHODS SHALL CONFORM WITH APPLICABLE FEDERAL, STATE, AND CITY OF PORTLAND CODES AND SPECIFICATIONS.
- COMPACTION REQUIREMENTS:

LOCATION	BELOW PAVED AREAS	BELOW GRASSED AREAS
MINIMUM COMPACTION*	95%	90%

*ALL PERCENTAGES OF COMPACTION SHALL BE OF MAXIMUM DRY DENSITY AT THE OPTIMUM MOISTURE CONTENT AS DETERMINED AND CONTROLLED IN ACCORDANCE WITH ASTM D-1557.
- CLEAN AND/OR FLUSH SEDIMENT AND DEBRIS FROM ALL MANHOLES, CATCH BASINS AND ASSOCIATED PIPING AFTER THE WORK HAS BEEN COMPLETED. ACCUMULATED SEDIMENT SHALL BE REMOVED AND PROPERLY DISPOSED OF.
- PROPOSED CATCH BASIN, SEWER MANHOLE (SMH), AND DRAIN MANHOLE (DMH) SYMBOLS REPRESENT LOCATION OF NEW STRUCTURES. REFER TO DETAILS AND SPECIFICATIONS FOR INFORMATION ON FRAMES AND GRATES, COVERS, SHAPE, STYLE AND DIMENSIONS.
- ALL WORK WITHIN CITY STREET RIGHTS-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH REQUIREMENTS OF THE CITY TRAFFIC ENGINEER. THE CONTRACTOR SHALL SUBMIT A PROPOSED TRAFFIC CONTROL PLAN TO THE TRAFFIC ENGINEER AT LEAST 7 DAYS BEFORE STARTING CONSTRUCTION IN ANY STREET. THE TRAFFIC CONTROL PLAN SHALL BE SUBJECT TO APPROVAL BY THE TRAFFIC ENGINEER, WHO MAY ATTACH SPECIAL CONDITIONS TO, OR REQUIRE MODIFICATIONS OF, THE TRAFFIC CONTROL PLAN. CONSTRUCTION SHALL NOT BEGIN UNTIL THE PLAN IS APPROVED BY THE CITY TRAFFIC ENGINEER.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN ALL NECESSARY BARRIERS, FENCES, LIGHTS, WARNING SIGNS AND OTHER DEVICES NECESSARY TO SAFEGUARD TRAFFIC AND THE PUBLIC DURING WORKING AND NON-WORKING HOURS FOR THE DURATION OF THE PROJECT. SITE SHALL BE LEFT WITH APPROPRIATE SAFETY MEASURES IN PLACE DURING NON-WORKING HOURS. NO TRENCH SHALL BE LEFT OPEN DURING NON-WORKING HOURS. SITE SAFETY IS THE RESPONSIBILITY OF THE CONTRACTOR, DURING BOTH WORKING AND NON-WORKING HOURS.
- IT IS THE CONTRACTORS RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH THE EXISTING CONDITIONS PRIOR TO BIDDING.
- ON ALL "ALTER", "MODIFY," "ABANDON" AND "REMOVE" STRUCTURES, THE CONTRACTOR SHALL REMOVE ABANDONED PIPES OR PLUG AND FILL WITH FLOWABLE FILL.
- THE COST OF REMOVING EXISTING STRUCTURES AND/OR PIPES SHALL BE INCIDENTAL TO THE COST OF THE REPLACEMENT STRUCTURE AND/OR PIPE WHEN REPLACED IN THE SAME VICINITY, REFER TO SPECIFICATION SECTION 202.08.
- RESTRICT ACCESS TO SITE THROUGH THE USE OF APPROPRIATE SIGNAGE, BARRIERS, FENCES, ETC. SITE SHALL BE LEFT WITH APPROPRIATE SAFETY MEASURES IN PLACE DURING NON-WORKING HOURS. NO TRENCH SHALL BE LEFT OPEN DURING NON-WORKING HOURS. SITE SAFETY IS THE RESPONSIBILITY OF THE CONTRACTOR, DURING BOTH WORKING AND NON-WORKING HOURS.
- THE FOLLOWING SHALL BE INCIDENTAL TO THE 603 & 604 PAY ITEMS: - CUTTING OF PIPE AND/OR CONNECTIONS NECESSARY TO CONSTRUCT NEW STORM DRAIN & SEWER PIPE AND APPURTENANCES - WORK & MATERIALS NECESSARY TO CONNECT NEW OR EXISTING PIPES TO CATCH BASINS OR MANHOLES -CHANGES TO FLOW LINES/PROFILE GRADES/PIPE INVERTS.
- AT LOCATIONS WHERE EXISTING CATCH BASINS ARE BEING REMOVED AND NEW CATCH BASINS ADDED IN A DIFFERENT LOCATION, CONTRACTOR SHALL REGRADE THE AREA WHERE THE OLD BASIN IS BEING REMOVED TO BLEND IN SMOOTHLY WITH ADJACENT STREET GRADES. AT THE LOCATION OF THE NEW CATCH BASIN, CONTRACTOR SHALL CONSTRUCT APPROPRIATE DEPRESSION PER THE CITY OF PORTLAND DETAILS AND STANDARDS.
- ALL SIDEWALK CONSTRUCTION SHALL CONFORM TO THE APPROPRIATE ADA/PROWAG STANDARDS, INCLUDING BUT NOT LIMITED TO THE FOLLOWING:
 - WALKWAYS SHALL HAVE A MAXIMUM CROSS SLOPE (PERPENDICULAR TO THE DIRECTION OF TRAVEL) OF 1.5% AND A MAXIMUM LONGITUDINAL SLOPE (ALONG THE DIRECTION OF TRAVEL) OF 5%, UNLESS LONGITUDINAL SLOPE OF ROADWAY IS STEEPER.
 - CURB RAMPS MAY HAVE A RUNNING SLOPE OF UP TO 8% (1" PER FOOT), BUT ARE NOT REQUIRED TO EXCEED 15' IN LENGTH. WHERE 8% RUNNING SLOPE IS NOTE POSSIBLE, CURB STONES ARE TO BE SET TO PROVIDE CONSTANT SLOPE OVER 15', ALONG TOP-OF-CURB. FROM FULL CURB REVEAL TO FLUSH CURB.
 - LANDING AREAS SHALL HAVE A MAXIMUM SLOPE OF 1.5% IN ALL DIRECTIONS.
 - IF THE CONTRACTOR DETERMINES THAT THE ABOVE STANDARDS ARE NOT COMPATIBLE WITH SITE CONDITIONS THEY SHALL CONTACT THE ENGINEER/RESIDENT INSPECTOR UPON DISCOVERY AND REQUEST GUIDANCE. INSTALLATION OF A RAMP OR SIDEWALK SEGMENT WITHOUT GUIDANCE FROM AND APPROVAL OF ENGINEER/RESIDENT, AND WHICH DOES NOT MEETING ADA STANDARDS WILL NOT BE ACCEPTED.
 - CONTRACTOR TO FIELD VERIFY LIMITS OF WORK AND GRADING OF DRIVEWAYS WITH ENGINEER/RESIDENT INSPECTOR PRIOR TO BEGINNING WORK. SAW CUTS TO BE MARKED OUT AND VERIFIED WITH ENGINEERING PRIOR TO CUTTING.

DESIGN BY: E. BOLT		DRAWN BY: L.A. SOEZA		CHECKED BY: K. GRAY		SHEET NAME: GENERAL NOTES		DATE: FEB 2024		SHEET NO.: 1	
PROJECT NAME: 2025 DOBG SIDEWALK AND ADA IMPROVEMENT PROJECT		CITY OF PORTLAND, MAINE DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION 212 CANO RD., SUITE B, PORTLAND, MAINE 04101 PHONE (207) 874-5801 FAX (207) 874-5852		SURVEY CREW:		DATE: FEB. 28, 2019		SUBMISSION		REV:	





PLAN NOTES:

- DURING CONSTRUCTION, THE CONTRACTOR NOR ANY SUBCONTRACTOR SHALL BE PERMITTED TO FOUL THE TRACKS WITH EQUIPMENT(S) OR PERSONNEL, UNLESS A RAILROAD FLAGGER IS PRESENT AND/OR THE WORK HAS BEEN AUTHORIZED BY CSX.
- IN CASE OF EMERGENCY DURING CONSTRUCTION AT THE GRADE CROSSING INVOLVING THE RAILROAD TRACKS OR THE RAILROAD WARNING DEVICES, PLEASE CONTACT THE EMERGENCY PHONE NUMBER FOR CSX @ 1-800-955-9208 (REF. CROSSING NUMBER: 837476N - WOODFORD STREET)
- ALL SIDEWALKS, INCLUDING CURB RAMPS, MUST MEET THE AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS. SEE 'GENERAL NOTES' ON SHEET 1. IF THE CONTRACTOR DETERMINES THAT ADA STANDARDS ARE NOT COMPATIBLE WITH SITE CONDITIONS, THEY SHALL CONTACT THE ENGINEER/RESIDENT IMMEDIATELY UPON DISCOVERY AND REQUEST GUIDANCE. INSTALLATION OF A RAMP OR SIDEWALK SEGMENT THAT DIFFERS FROM THE PLAN WITHOUT GUIDANCE FROM AND APPROVAL OF ENGINEER/RESIDENT, AND WHICH DOES NOT MEET ADA STANDARDS WILL NOT BE ACCEPTED.
- SIDEWALK CROSS-SLOPE SHALL BE NO LESS THAN 0.9% AND NO MORE THAN 2.0%. CONTRACTOR SHOULD AIM FOR 1.5% TO ALLOW FOR CONSTRUCTION TOLERANCE.
- SIDEWALK AND DRIVEWAY RECONSTRUCTION MATERIAL TO FOLLOW CITY'S SIDEWALK MATERIAL POLICY UNLESS OTHERWISE NOTED.
- CONTRACTOR TO COORDINATE WITH PROPERTY OWNER IN ADVANCE OF ANY DRIVEWAY WORK.
- CONTRACTOR TO CONFIRM INVERTS IN FIELD PRIOR TO ORDERING STRUCTURES.
- GRADE ESPLANADE TO SLOPE AWAY FROM SIDEWALK TO EXISTING TOP OF CURB, UNLESS OTHERWISE NOTED. EXISTING GRADES SHOULD BE MAINTAINED WHERE POSSIBLE NEAR TREES TO PROTECT ROOTS.
- PROPOSED SIDEWALK TO BE NO LESS THAN 5' WIDE UNLESS OTHERWISE NOTED.
- CONTRACTOR TO PROTECT TREES NOT OTHERWISE LABELED FOR REMOVAL.
- PROTECTION OF EXISTING TREES - INCLUDING METHODS OF EXCAVATION, TREATMENT OF EXPOSED ROOTS, AND GENERAL CONSTRUCTION PRACTICES FOR WORK IN CLOSE PROXIMITY TO THE TREES - SHALL BE IN ACCORDANCE WITH SECTION 622 OF THE SPECIFICATIONS.
- ANY EXISTING FLEXIBLE SIGNS MOUNTED ON UTILITY POSTS WITHIN THE PROJECT LIMIT ARE TO BE REPLACED WITH CITY STANDARD U-CHANNEL POSTS AND TRADITIONAL SIGNS.
- ANY SIGNAGE/POSTS LOCATED WITHIN THE WORK AREA AND NEEDING TO BE REMOVED SHALL BE RESET IN EXISTING LOCATION, UNLESS OTHERWISE NOTED OR DIRECTED BY RESIDENT INSPECTOR.
- CONTRACTOR TO NOTIFY PROJECT MANAGER WITH APPROXIMATE QUANTITY OF WORK IF INDETERMINATE CONTRACT ITEMS ARE ENCOUNTERED IN THE FIELD.
- A 1/4-INCH THICK PREFORMED JOINT FILLER SHALL BE USED AGAINST ANY FIXED OBJECTS WITHIN OR DIRECTLY ADJACENT TO THE CONCRETE SIDEWALK POUR.
- THE CONTRACTOR SHALL COORDINATE WITH THE PROJECT MANAGER AND RESIDENT INSPECTOR DURING LAYOUT, PRIOR TO THE START OF EARTHWORK ACTIVITIES.

LEGEND

EXISTING CONDITIONS		PROPOSED CONDITIONS	
GRANITE MONUMENT SIGN		DECIDUOUS TREE	
DECIDUOUS TREE		CATCH BASIN	
STREET LINE		DOUBLE MOUNTED RRFB	
PROPERTY LINE		ELECTRICAL CONDUIT	
5' CONTOUR INTERVAL		STORM DRAIN	
1' CONTOUR INTERVAL		NEW GRANITE CURB	
DRAIN LINE		RESET GRANITE CURB	
ELECTRIC LINE		NEW GRANITE TIPDOWN CURB	
SEWER LINE		SAW CUT	
TELEPHONE LINE		NEW CONCRETE SIDEWALK	
WATER LINE		DETECTABLE WARNING PANEL (DWP)	
GRANITE CURB		ADA RAMP LANDING AREA	
EDGE OF ROAD / DRIVEWAY / PARKING LOT SIDEWALK		LOAM & SEED AREA	

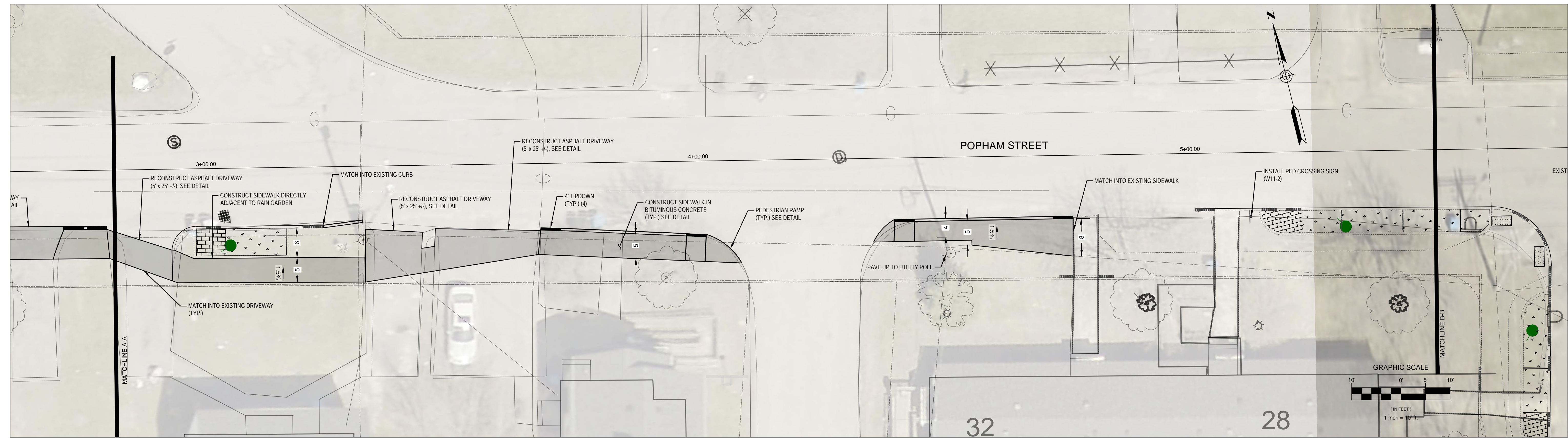
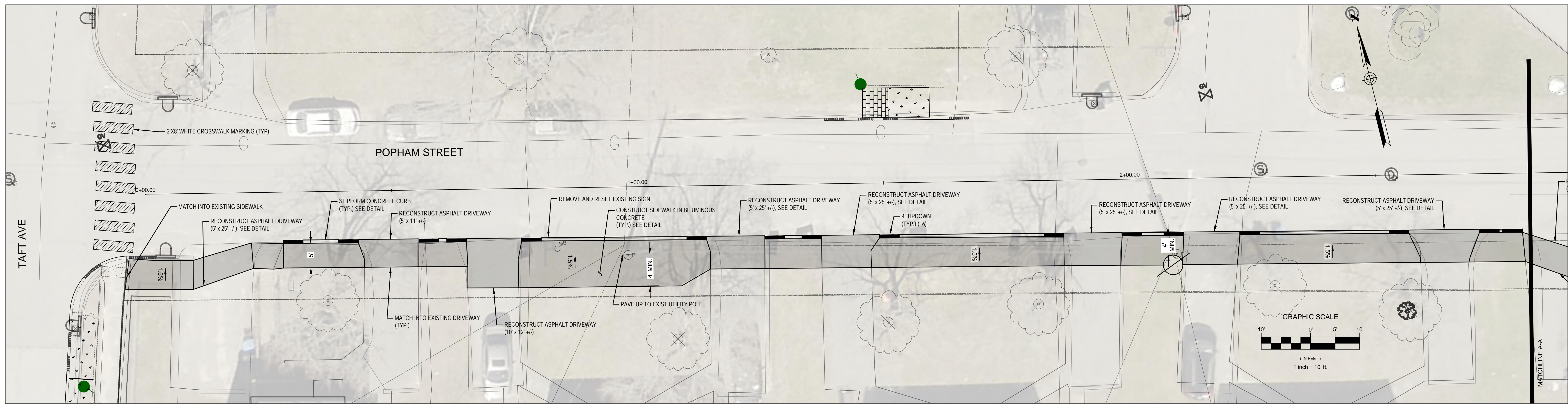
DATE	12/16/22
SUBMISSION	1
BID SET	2
PLAN NOTE CHANGE	1/25

PROJECT NAME:
2025 CDBG SIDEWALK AND ADA IMPROVEMENT
 CITY OF PORTLAND, MAINE
 DEPARTMENT OF PUBLIC WORKS
 DIVISION
 212 CANCO ROAD, SUITE B, PORTLAND, MAINE 04103
 PHONE (207) 874-8846



DESIGN BY:	EJB
DRAWN BY:	EJB
CHECKED BY:	JWL
SHEET NAME:	SITE PLAN WOODFORD STREET
DATE:	SEPT 2024
SHEET NO.:	10
VAULT #:	01047_050

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EXISTING CONDITIONS	PROPOSED CONDITIONS
GRANITE MONUMENT	DECIDUOUS TREE
SIGN	CATCH BASIN
DECIDUOUS TREE	DOUBLE MOUNTED RRFB
STREET LINE	ELECTRICAL CONDUIT
PROPERTY LINE	STORM DRAIN
5' CONTOUR INTERVAL	NEW GRANITE CURB
1' CONTOUR INTERVAL	NEW GRANITE TIPDOWN CURB
DRAIN LINE	SAW CUT
ELECTRIC LINE	NEW ASPHALT SIDEWALK
SEWER LINE	DETECTABLE WARNING PANEL (DWP)
TELEPHONE LINE	ADA RAMP LANDING AREA
WATER LINE	LOAM & SEED AREA
FIRE ALARM	
EDGE OF ROAD / DRIVEWAY / PARKING LOT	
SIDEWALK	

- NOTES:**
- ALL SIDEWALKS, INCLUDING CURB RAMPS, MUST MEET THE AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS. SEE 'GENERAL NOTES' ON SHEET 1. IF THE CONTRACTOR DETERMINES THAT ADA STANDARDS ARE NOT COMPATIBLE WITH SITE CONDITIONS, THEY SHALL CONTACT THE ENGINEER/RESIDENT IMMEDIATELY UPON DISCOVERY AND REQUEST GUIDANCE. INSTALLATION OF A RAMP OR SIDEWALK SEGMENT THAT DIFFERS FROM THE PLAN WITHOUT GUIDANCE FROM AND APPROVAL OF ENGINEER/RESIDENT, AND WHICH DOES NOT MEET ADA STANDARDS WILL NOT BE ACCEPTED.
 - SIDEWALK CROSS-SLOPE SHALL BE NO LESS THAN 0.9% AND NO MORE THAN 2.0%. CONTRACTOR SHOULD AIM FOR 1.5% TO ALLOW FOR CONSTRUCTION TOLERANCE.
 - CONTRACTOR TO NOTIFY PROJECT MANAGER WITH APPROXIMATE QUANTITY OF WORK IF INDETERMINATE CONTRACT ITEMS ARE ENCOUNTERED IN THE FIELD.
 - SIDEWALK AND DRIVEWAY RECONSTRUCTION MATERIAL TO FOLLOW CITY'S SIDEWALK MATERIAL POLICY UNLESS OTHERWISE NOTED.
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 - CONTRACTOR TO CONFIRM INVERTS IN FIELD PRIOR TO ORDERING STRUCTURES.
 - GRADE ESPLANADE TO SLOPE AWAY FROM SIDEWALK TO EXISTING TOP OF CURB, UNLESS NOTED OTHERWISE. EXISTING GRADES SHOULD BE MAINTAINED WHERE POSSIBLE NEAR TREES TO PROTECT ROOTS.
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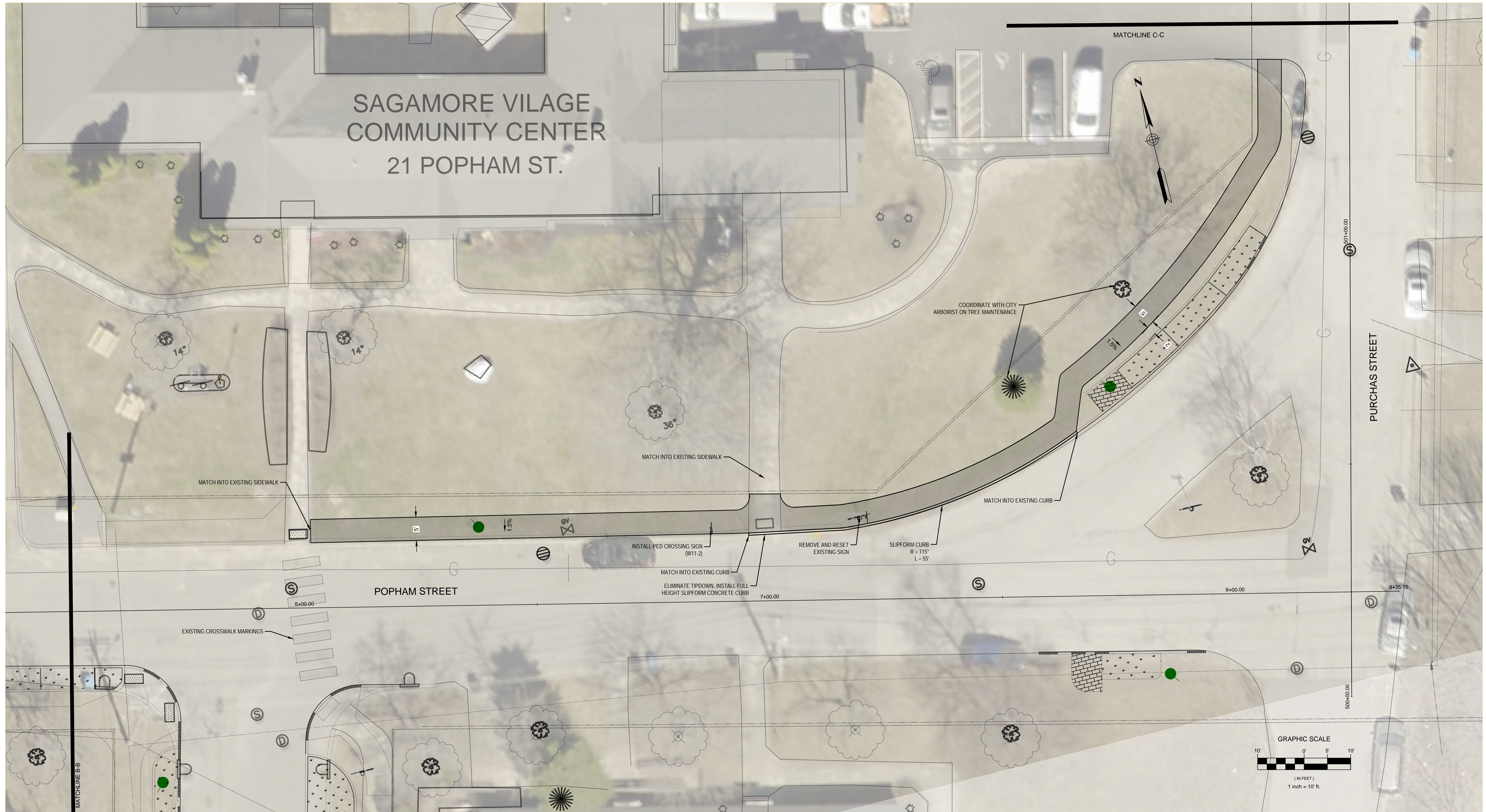
NO.	DESCRIPTION	DATE
1	BID SET	12/16/22
	SUBMISSION	

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 DEPARTMENT OF PUBLIC WORKS
 DIVISION
 212 CANCO ROAD, SUITE B, PORTLAND, MAINE 04103
 PHONE (207) 874-8846



DESIGN BY:	EJB
DRAWN BY:	EJB
CHECKED BY:	JWL
SHEET NAME:	SITE PLAN 1 SAGAMORE VILLAGE
DATE:	SEPT 2024
SHEET NO.:	11
VAULT #:	01047_050

SAGAMORE VILAGE COMMUNITY CENTER 21 POPHAM ST.



LEGEND

EXISTING CONDITIONS	PROPOSED CONDITIONS
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DECIDUOUS TREE	CATCH BASIN
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SIDEWALK	

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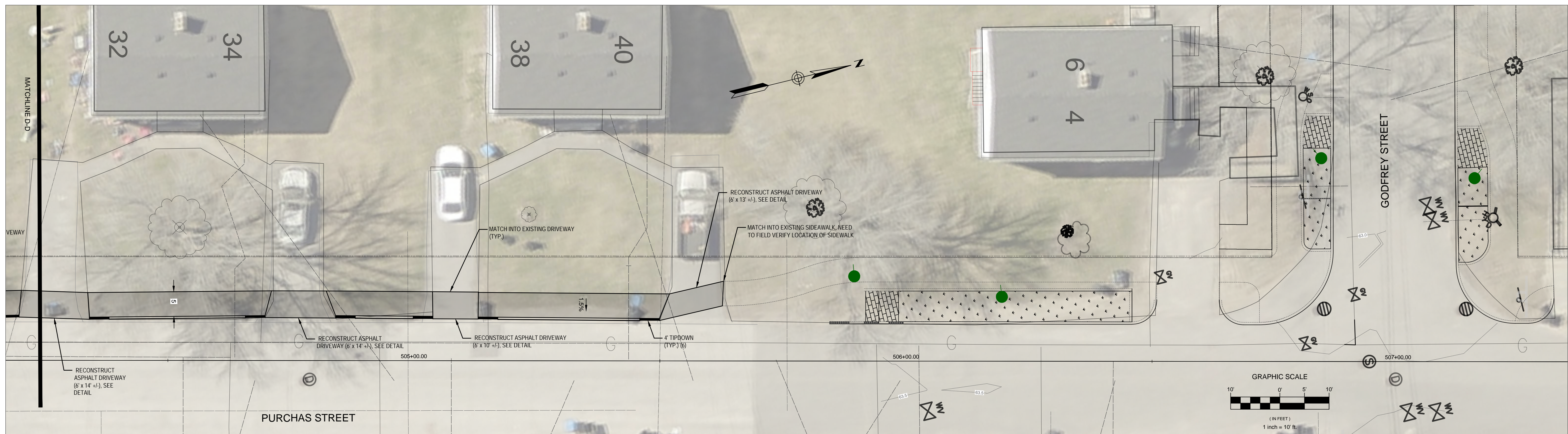
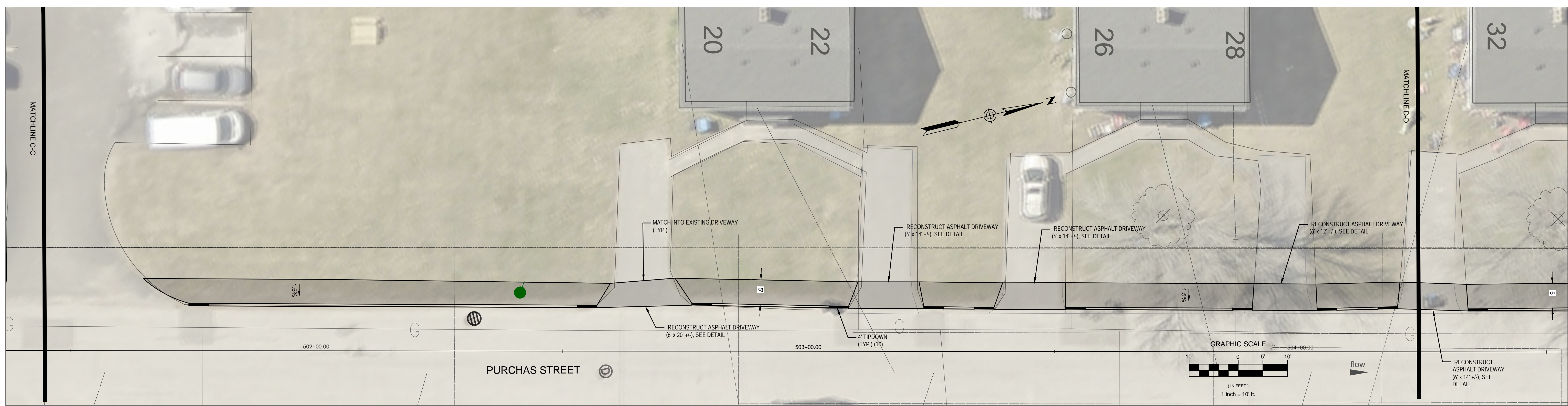
PROJECT NAME:
**2025 CDBG SIDEWALK
 AND ADA IMPROVEMENT**
 CITY OF PORTLAND, MAINE
 DEPARTMENT OF PUBLIC WORKS
 DIVISION
 212 CANCO ROAD, SUITE B, PORTLAND, MAINE 04103
 PHONE (207) 874-8846



DESIGN BY:	EJB
DRAWN BY:	EJB
CHECKED BY:	JWL
SHEET NAME:	SITE PLAN 2 SAGAMORE VILLAGE
DATE:	JULY 2024
SHEET NO.:	12
VAULT #:	01047_050

REV	SUBMISSION	DATE
1	BID SET	12/16/22

I:\SHARED_DRIVE\DRY - PROJECTS\PROJECTS\2024\07_SIDEWALK_PROJECTS\DESIGN\DRAWINGS\CORNER_2024\DWG\SAGAMORE_VILAGE_SIDEWALK_DESIGN.DWG 36X24 GRADING PLAN 2 2/9/2024



LEGEND

<u>EXISTING CONDITIONS</u>		<u>PROPOSED CONDITIONS</u>	
GRANITE MONUMENT		DECIDUOUS TREE	
SIGN		CATCH BASIN	
DECIDUOUS TREE		DOUBLE MOUNTED RRFB	
STREET LINE		ELECTRICAL CONDUIT	
PROPERTY LINE		STORM DRAIN	
5' CONTOUR INTERVAL		NEW GRANITE CURB	
1' CONTOUR INTERVAL		NEW GRANITE TIPDOWN CURB	
DRAIN LINE		SAW CUT	
ELECTRIC LINE		NEW ASPHALT SIDEWALK	
SEWER LINE		DETECTABLE WARNING PANEL (DWP)	
TELEPHONE LINE		ADA RAMP LANDING AREA	
WATER LINE		LOAM & SEED AREA	
FIRE ALARM			
GRANITE CURB			
EDGE OF ROAD / DRIVEWAY / PARKING LOT			
SIDEWALK			

NOTES:

- ALL SIDEWALKS, INCLUDING CURB RAMPS, MUST MEET THE AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS. SEE 'GENERAL NOTES' ON SHEET 1. IF THE CONTRACTOR DETERMINES THAT ADA STANDARDS ARE NOT COMPATIBLE WITH SITE CONDITIONS, THEY SHALL CONTACT THE ENGINEER/RESIDENT IMMEDIATELY UPON DISCOVERY AND REQUEST GUIDANCE. INSTALLATION OF A RAMP OR SIDEWALK SEGMENT THAT DIFFERS FROM THE PLAN WITHOUT GUIDANCE FROM AND APPROVAL OF ENGINEER/RESIDENT, AND WHICH DOES NOT MEET ADA STANDARDS WILL NOT BE ACCEPTED.
- SIDEWALK CROSS-SLOPE SHALL BE NO LESS THAN 0.9% AND NO MORE THAN 2.0%. CONTRACTOR SHOULD AIM FOR 1.5% TO ALLOW FOR CONSTRUCTION TOLERANCE.
- CONTRACTOR TO NOTIFY PROJECT MANAGER WITH APPROXIMATE QUANTITY OF WORK IF INDETERMINATE CONTRACT ITEMS ARE ENCOUNTERED IN THE FIELD.
- SIDEWALK AND DRIVEWAY RECONSTRUCTION MATERIAL TO FOLLOW CITY'S SIDEWALK MATERIAL POLICY UNLESS OTHERWISE NOTED.
- CONTRACTOR TO COORDINATE WITH PROPERTY OWNER IN ADVANCE OF ANY DRIVEWAY WORK.
- CONTRACTOR TO CONFIRM INVERTS IN FIELD PRIOR TO ORDERING STRUCTURES.
- GRADE ESPLANADE TO SLOPE AWAY FROM SIDEWALK TO EXISTING TOP OF CURB, UNLESS NOTED OTHERWISE. EXISTING GRADES SHOULD BE MAINTAINED WHERE POSSIBLE NEAR TREES TO PROTECT ROOTS.
- PROPOSED SIDEWALK TO BE NO LESS THAN 5' WIDE UNLESS NOTED OTHERWISE.
- CONTRACTOR TO PROTECT ANY EXISTING RETAINING WALL ADJACENT TO PROPOSED SIDEWALK WORK.
- CONTRACTOR TO PROTECT TREES NOT OTHERWISE LABELED FOR REMOVAL.
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- A 1/4-INCH THICK PREFORMED JOINT FILLER SHALL BE USED AGAINST ANY FIXED OBJECTS WITHIN OR DIRECTLY ADJACENT TO THE CONCRETE SIDEWALK POUR.

REV	DATE
1	12/16/22
	BID SET
	SUBMISSION

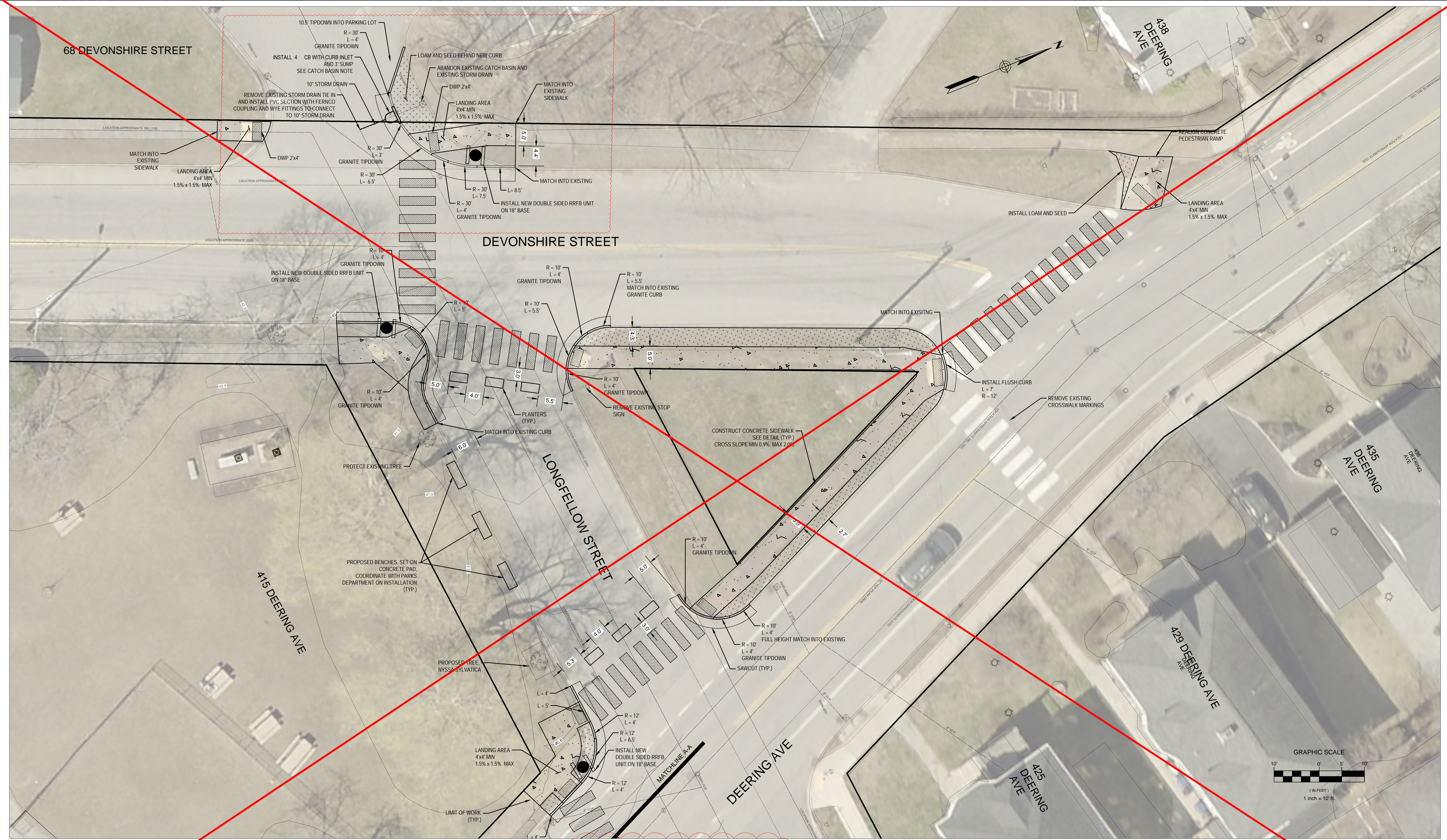
PROJECT NAME:
2025 CDBG SIDEWALK AND ADA IMPROVEMENT
 CITY OF PORTLAND, MAINE
 DEPARTMENT OF PUBLIC WORKS
 DIVISION
 212 CANCO ROAD, SUITE B, PORTLAND, MAINE 04103
 PHONE (207) 874-8846



DESIGN BY:	EJB
DRAWN BY:	EJB
CHECKED BY:	KG
SHEET NAME:	SITE PLAN 3 SAGAMORE VILLAGE
DATE:	JULY 2024
SHEET NO.:	13
VAULT #:	01047_050

I:\SHARED\DRIVEWAY - PROJECTS\PROJECTS\2024\07_SIDEWALK PROJECTS\DESIGN\DIVISIONS\CORNER_2024\DWG\SAGAMORE VILLAGE_SIDEWALK_DESIGN.DWG 38X24 GRADING PLAN 3 2/9/2024

I:\SHARED_DRIVE\DRM - PROJECTS\PROJECTS\02 - ACTIVE\2024\07 SIDEWALK PROJECTS\DESIGN\CAD\KIDDALE_ADA_CDDB_2024\DWG\KIDDALE DESIGN\NEW DWG 38\324 GRADING PLAN 1 2/29/2024



EXISTING CONDITIONS	
GRANITE MONUMENT	
SIGN	
DECIDUOUS TREE	
STREET LINE	
PROPERTY LINE	
5' CONTOUR INTERVAL	
1' CONTOUR INTERVAL	
DRAIN LINE	
ELECTRIC LINE	
SEWER LINE	
TELEPHONE LINE	
WATER LINE	
FIRE ALARM	
GRANITE CURB	
EDGE OF ROAD / DRIVEWAY / PARKING LOT	
SIDEWALK	

PROPOSED CONDITIONS	
DECIDUOUS TREE	
CATCH BASIN	
DOUBLE MOUNTED RRFB	
ELECTRICAL CONDUIT	
STORM DRAIN	
NEW GRANITE CURB	
NEW GRANITE TIPDOWN CURB	
SAW CUT	
NEW CONCRETE SIDEWALK	
DETECTABLE WARNING PANEL (DWP)	
ADA RAMP LANDING AREA	
LOAM & SEED AREA	

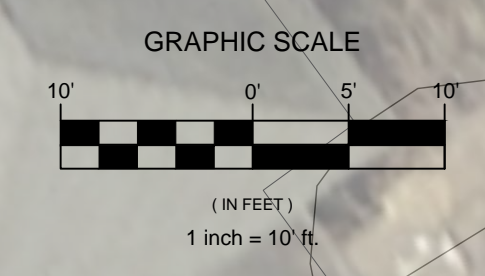
CATCH BASIN NOTES:

- TEST PIT SHALL BE PERFORMED BEFORE CATCH BASIN IS ORDERED TO VERIFY LOCATION AND DEPTH OF EXISTING STORM DRAIN.
- INVERT DEPTH SHALL BE DERIVED FROM TEST PIT RESULTS.

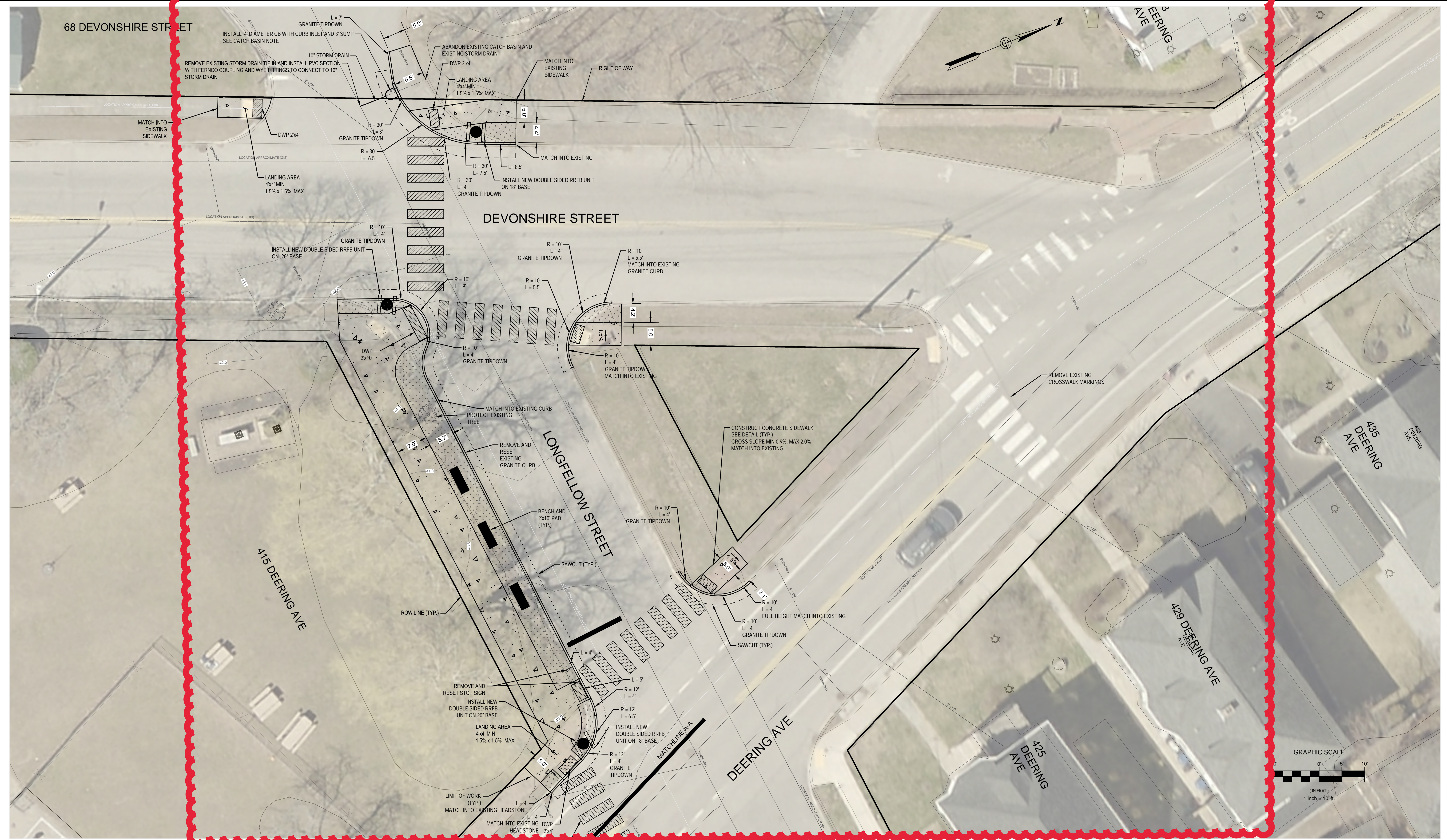
NOTES:

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PROJECT NAME:		2025 CDBG SIDEWALK AND ADA IMPROVEMENT	
CITY OF PORTLAND, MAINE		DEPARTMENT OF PUBLIC WORKS	
DIVISION		212 CANCO ROAD, SUITE B, PORTLAND, MAINE 04103	
PHONE (207) 874-8846			
DESIGN BY:	EJB	DATE:	SEPT 2024
DRAWN BY:	EJB	SHEET NO.:	14
CHECKED BY:		REV.:	
SHEET NAME:		SITE PLAN OAKDALE NEIGHBORHOOD	
VAULT #: 01047_050			



I:\SHARED_DRIVE\DRM - PROJECTS\PROJECTS03 - ACTIVE\2024\07 SIDEWALK PROJECTS\DESIGN\CAD\KIDDALE_JDA_CDGB_2024\DWG\OKDALE DESIGN-OPEN LONGFELLOW.DWG 38X24 GRADING PLAN 1 2/2024



EXISTING CONDITIONS

GRANITE MONUMENT	
SIGN	
DECIDUOUS TREE	
STREET LINE	
PROPERTY LINE	
5' CONTOUR INTERVAL	
1' CONTOUR INTERVAL	
DRAIN LINE	
ELECTRIC LINE	
SEWER LINE	
TELEPHONE LINE	
WATER LINE	
FIRE ALARM	
GRANITE CURB	
EDGE OF ROAD / DRIVEWAY / PARKING LOT	
SIDEWALK	

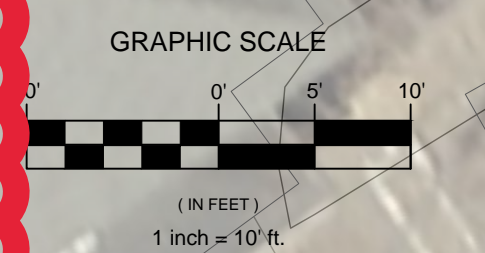
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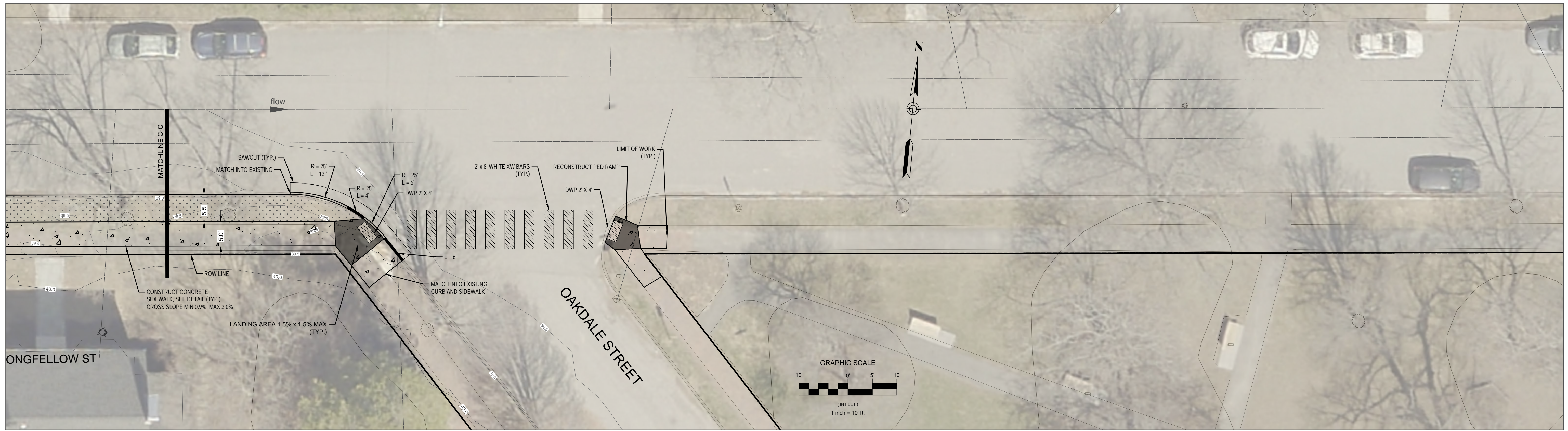


	6/2025	3/2025	1/25	12/2024	DATE
4	LAYOUT REVISION 2	3	LAYOUT REVISION	2	PLAN NOTE CHANGES
1	REV	1	REV	1	REV

PROJECT NAME: **2025 CDBG SIDEWALK AND ADA IMPROVEMENT**
 CITY OF PORTLAND, MAINE
 DEPARTMENT OF PUBLIC WORKS
 DIVISION
 212 CANCO ROAD, SUITE B, PORTLAND, MAINE 04103
 PHONE (207) 874-8846

DESIGN BY:	EJB
DRAWN BY:	EJB
CHECKED BY:	
SHEET NAME:	SITE PLAN OKDALE NEIGHBORHOOD
DATE:	SEPT 2024
SHEET NO.:	14
VAULT #:	01047_050

I:\SHARED DRIVES\DRIVE\PROJECTS\PROJECTS\2024\07 SIDEWALK PROJECTS\DESIGN\OAKDALE_ADA_CDDB_2024\DWG\OAKDALE DESIGN-NEW.DWG 38X24
 GRADING PLAN 3 2/2024



EXISTING CONDITIONS	
GRANITE MONUMENT	
SIGN	
DECIDUOUS TREE	
STREET LINE	
PROPERTY LINE	
5' CONTOUR INTERVAL	
1' CONTOUR INTERVAL	
DRAIN LINE	
ELECTRIC LINE	
SEWER LINE	
TELEPHONE LINE	
WATER LINE	
FIRE ALARM	
GRANITE CURB	
EDGE OF ROAD / DRIVEWAY / PARKING LOT	
SIDEWALK	

PROPOSED CONDITIONS	
DECIDUOUS TREE	
CATCH BASIN	
DOUBLE MOUNTED RRFB	
ELECTRICAL CONDUIT	
STORM DRAIN	
NEW GRANITE CURB	
NEW GRANITE TIPDOWN CURB	
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DETECTABLE WARNING PANEL (DWP)	
ADA RAMP LANDING AREA	
LOAM & SEED AREA	

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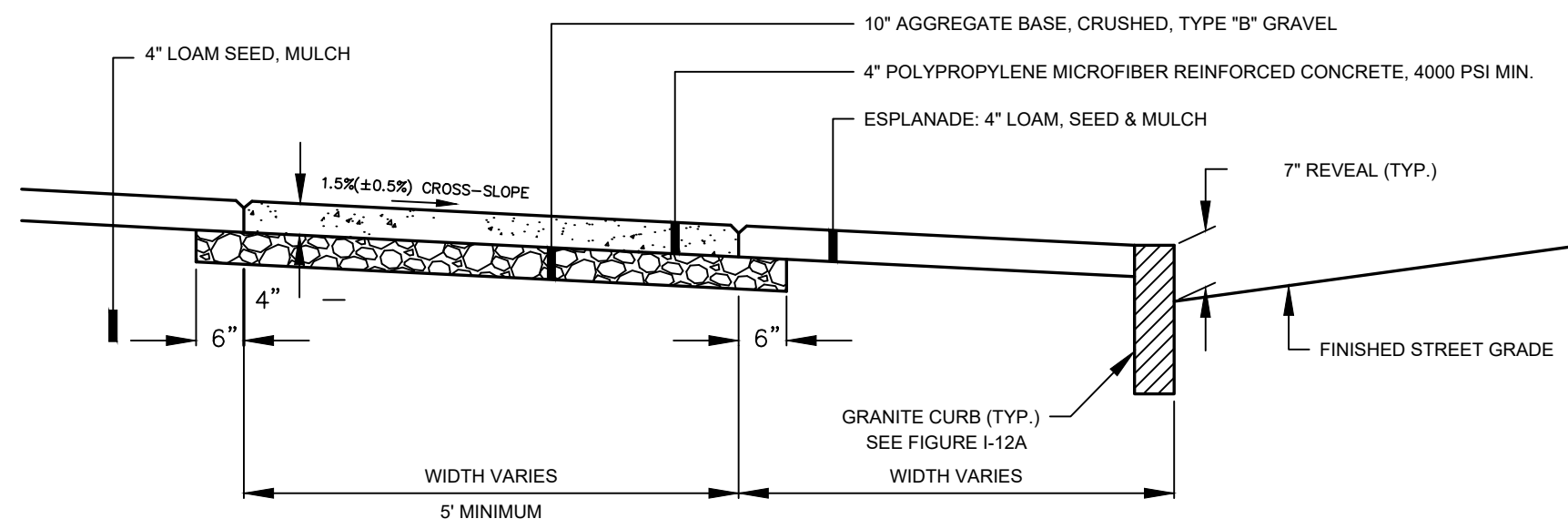
REV	DESCRIPTION	DATE
1	BID SET	12/16/22
	SUBMISSION	

PROJECT NAME:
2025 CDBG SIDEWALK AND ADA IMPROVEMENT
CITY OF PORTLAND, MAINE
 DEPARTMENT OF PUBLIC WORKS
 DIVISION
 212 CANCO ROAD, SUITE B, PORTLAND, MAINE 04103
 PHONE (207) 874-8846

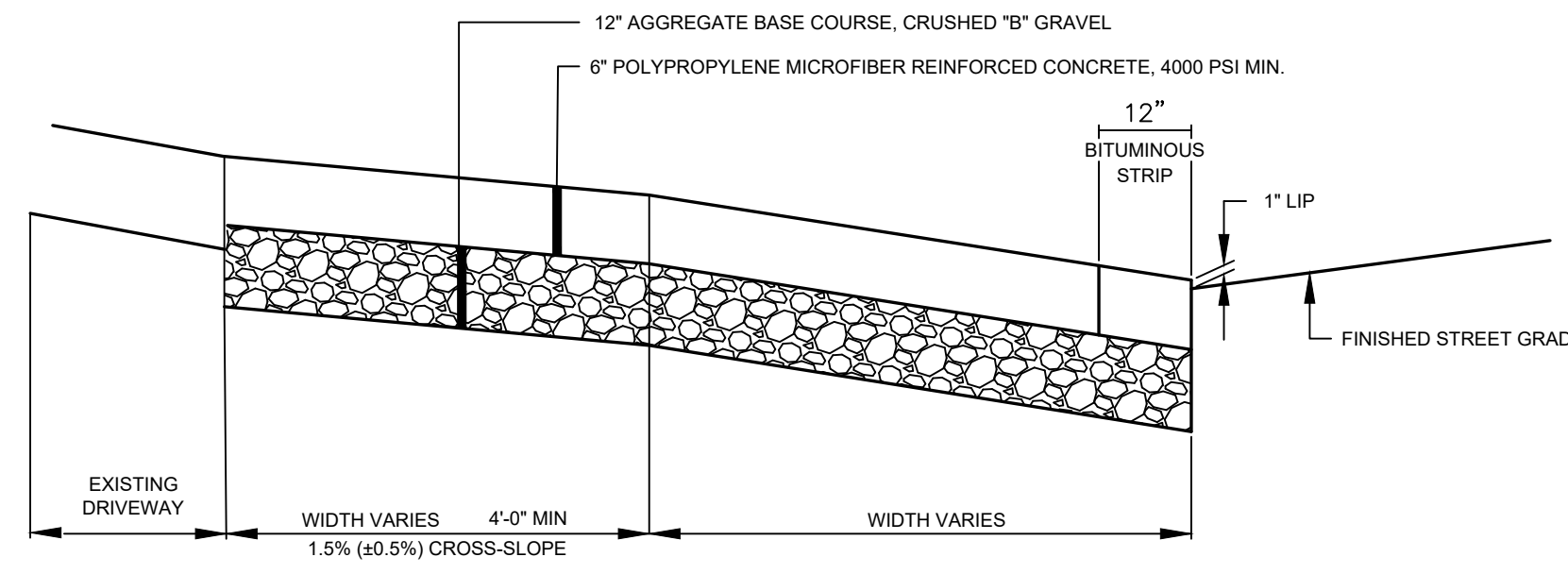


DESIGN BY:	EJB
DRAWN BY:	EJB
CHECKED BY:	
SHEET NAME: SITE PLAN 3 OAKDALE NEIGHBORHOOD	
DATE:	SEPT 2024
SHEET NO.:	16
VAULT #:	01047_050

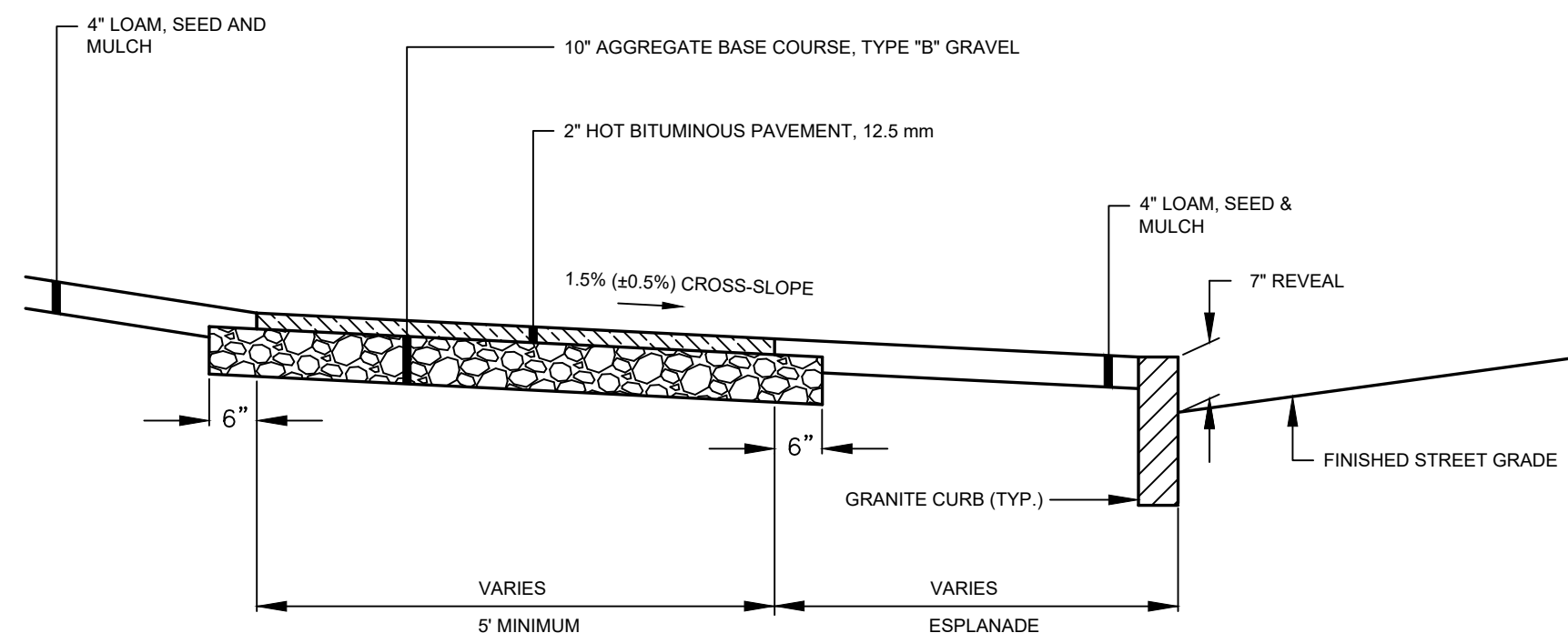
NOTE
CONCRETE SIDEWALKS TO BE TREATED WITH CONSOLIDECK SALTGUARD WB OR APPROVED EQUAL. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR APPLICATION.



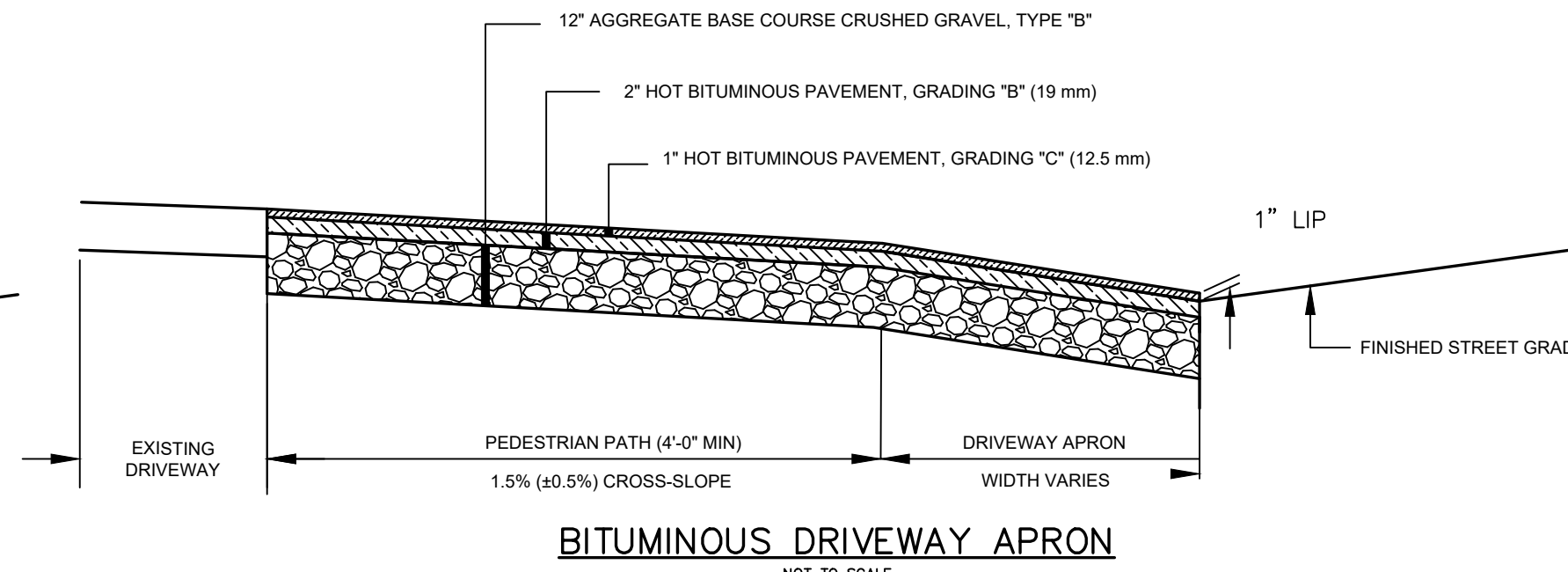
REINFORCED CONCRETE SIDEWALK
NOT TO SCALE



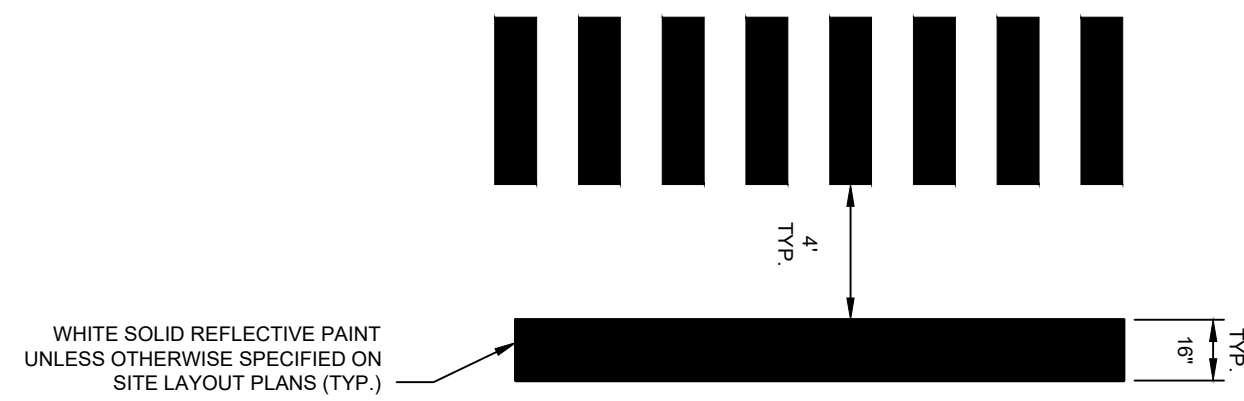
REINFORCED CONCRETE DRIVEWAY APRON
NOT TO SCALE



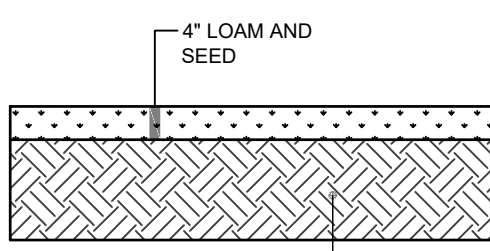
BITUMINOUS SIDEWALK
NOT TO SCALE



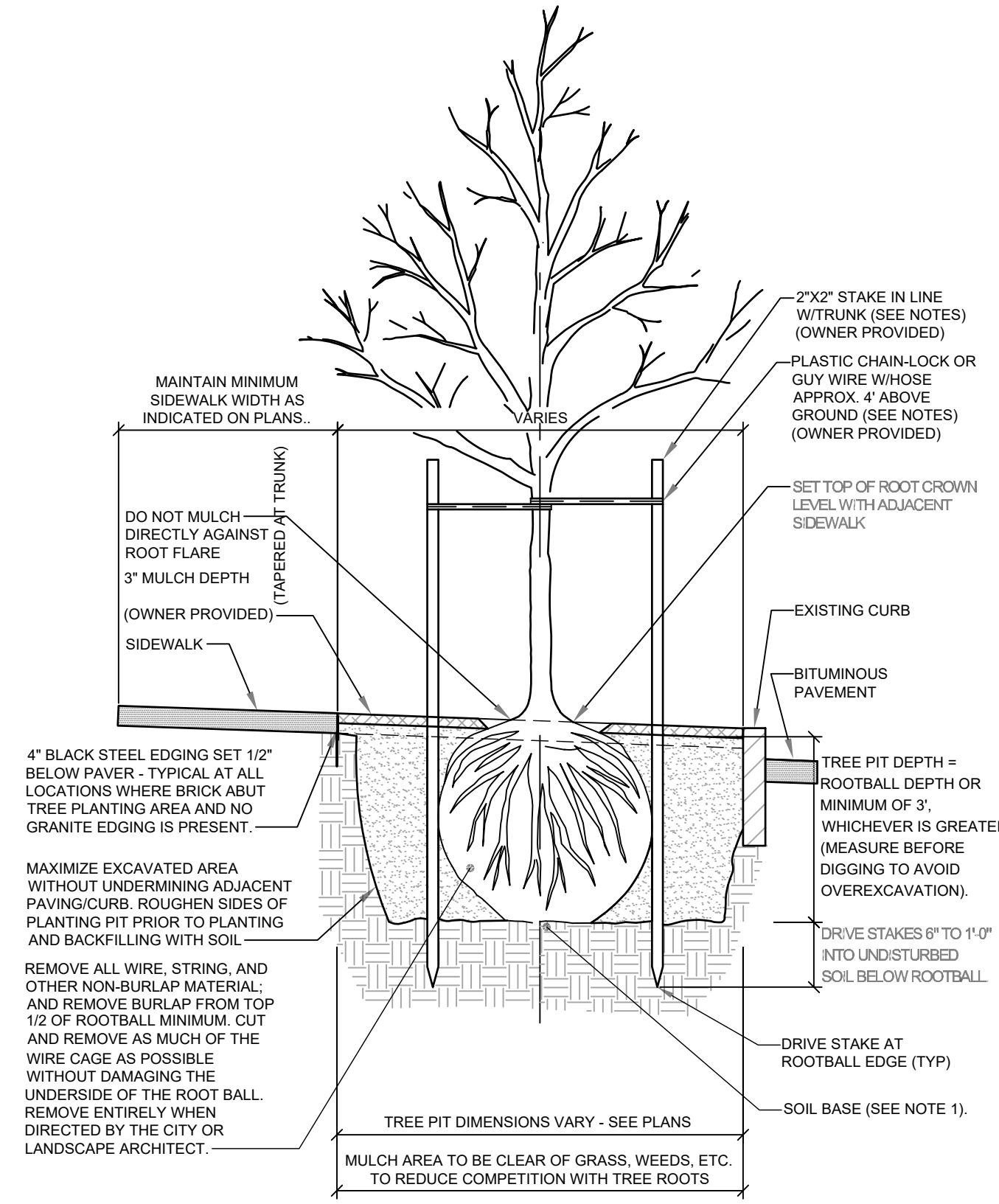
BITUMINOUS DRIVEWAY APRON
NOT TO SCALE



STOP BAR STRIPING
NOT TO SCALE



LOAM AND SEED DETAIL
NOT TO SCALE



TREE WELL / PLANTING DETAIL
NOT TO SCALE

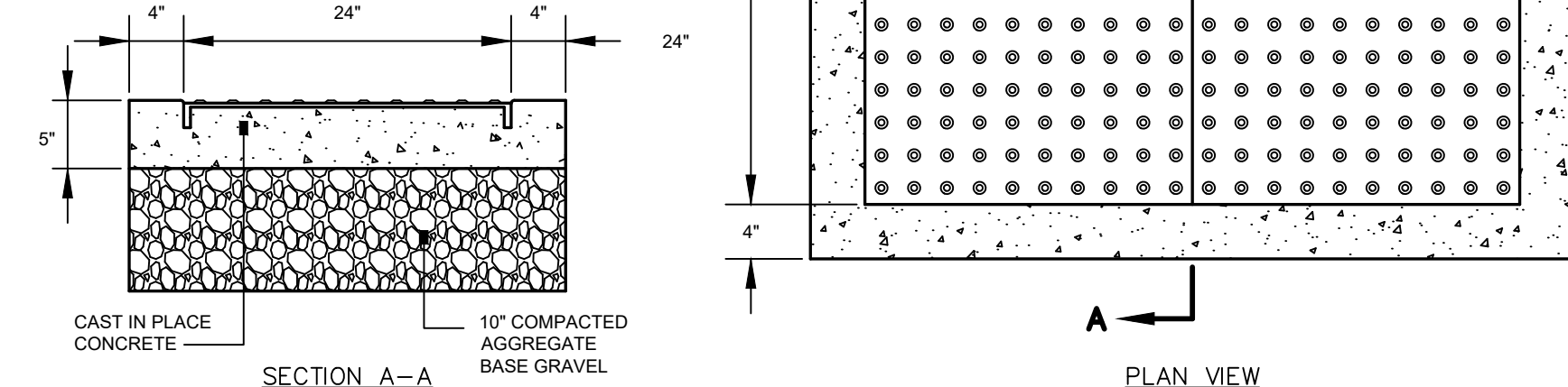
- NOTES:
- IN ACCORDANCE WITH ASTM STANDARDS REFERENCED IN THE NOTES, DURING SITE PREP, THE TREE WELL SHALL BE EXCAVATED TO A DEPTH OF 3' AND BACKFILLED WITH PLANTING SOIL WHEN READY FOR PLANTING. THE PLANTING PIT SHALL BE EXCAVATED AND THE ROOTBALL WILL BE SET ON A COMPACTED SOIL BASE, NOT UNDISTURBED SUBGRADE.
 - SEE DRAWING PLANS FOR TREE WELL DETAILS AND GRANITE EDGING DETAILS.
 - REMOVE EXISTING SOIL AND REPLACE WITH CITY PROVIDED SOIL MIX.
 - CONTRACTOR TO EXCAVATE AND REPLACE ALL SOILS TO A DEPTH OF 3' FOR ALL EXISTING AND NEW TREE WELLS, NEW ESPLANADES, AND EXISTING BLIMPOTS.
 - CONTRACTOR SHALL NOT EXCAVATE WITHIN 2' OF EXISTING UTILITY POLES, LIGHT POLES OR TRAFFIC SIGNALS.
 - EXISTING ESPLANADES AND RAISED PLANTERS TO RECEIVE NEW PLANTING SOIL TO A DEPTH OF 3' AND EQUIVALENT WIDTH TO ACHIEVE A MINIMUM OF 65 CUBIC FT. FOR EACH NEW TREE PIT.

GENERAL NOTES

- SIDEWALKS SHALL BE CONSTRUCTED IN BRICK, CONCRETE, OR BITUMINOUS, ACCORDING TO THE SIDEWALK & DRIVEWAY APRON MATERIALS POLICY, UNLESS DIRECTED OTHERWISE BY THE CITY ENGINEER.
- SIDEWALKS AND RAMPS SHALL BE BUILT ACCORDING TO THE FOLLOWING DIMENSIONS AND GRADES:
SIDEWALK RUNNING SLOPE: MATCH ADJACENT STREET GRADE
SIDEWALK CROSS SLOPE: 1.0% MINIMUM, 2.0% MAXIMUM
SIDEWALK WIDTH: 5'-0" MINIMUM, WIDER PREFERRED
ESPLANADE (WHERE PROVIDED) 2'-0" MINIMUM, 4'-0" OR GREATER PREFERRED IF LANDSCAPED.
- TWO CURB RAMPS ARE REQUIRED AT EACH SIDEWALK CORNER. EACH CURB RAMP SHALL CONNECT THE PEDESTRIAN ACCESS ROUTE AT EACH PEDESTRIAN STREET CROSSING. IN ALTERATIONS WHERE PHYSICAL CONSTRAINTS PREVENT TWO CURB RAMPS FROM BEING INSTALLED AT A STREET CORNER, A SINGLE PEDESTRIAN CURB RAMP MAY BE PERMITTED.
- OPPOSING CURB RAMPS AT A SINGLE CROSSING SHALL ALIGN TO PROVIDE A STRAIGHT PATH OF TRAVEL FROM THE TOP OF THE RAMP TO THE CURB RAMP ON THE OPPOSITE SIDE OF THE ROADWAY TO THE MAXIMUM EXTENT FEASIBLE.
- ALL SIDEWALKS AND RAMPS SHALL DRAIN TOWARD THE STREET, DRIVEWAY, OR PARKING AREA, WITH A MAXIMUM CROSS SLOPE OF 2.0%.
- LEVEL LANDINGS (NO GREATER THAN 2.0% SLOPE IN ANY DIRECTION) AT THE BOTTOM OF PERPENDICULAR RAMPS SHALL BE WHOLLY CONTAINED WITHIN MARKED CROSSWALKS.
- PONDING SHALL NOT BE PERMITTED WITHIN THE CURB RAMP LIMITS. RAMPS SHALL NOT ALTER ROADWAY DRAINAGE PATTERNS.
- TRANSITION FROM RAMPS TO WALKS OR STREET SURFACE SHALL BE FLUSH AND FREE OF ABRUPT CHANGES. PAVEMENT AT THE STREET SURFACE SHALL BE MILLED TO ACHIEVE FLUSH CONDITION.
- CURB RAMP AND FORM WORK SLOPES SHALL BE CHECKED WITH A DIGITAL LEVEL OF AN APPROPRIATE LENGTH. NO PORTION OF A RAMP RUN SHALL EXCEED THE MAXIMUM SLOPE REQUIREMENT.
- THE COUNTER SLOPE WITHIN 2' OF THE CURB RAMP SHALL BE 5% MAXIMUM. IN ALTERATIONS, IF THE COUNTER SLOPE OF 5% MAXIMUM CANNOT BE ACHIEVED, THE SLOPE ELEVATION MAY BE ADJUSTED SO THAT THE COMBINED COUNTER SLOPE AND RAMP SLOPE DO NOT EXCEED 13.3%.
- VERTICAL DROP-OFF EDGES TO RAMPS SHALL NOT BE BUILT UNLESS THE RAMP ABUTS AN ESPLANADE OR OTHER AREA NOT TO BE USED BY PEDESTRIANS.
- AT MARKED CROSSWALKS, THE FULL WIDTH OF THE RAMP OR LANDING SHALL BE CONTAINED WITHIN THE PAVEMENT MARKINGS.
- TACTILE WARNING PANELS SHALL BE PROVIDED ON ALL RAMPS AS PER FIGURE I-7 (OR I-7A) OF THESE STANDARD DETAILS.
- RAMP FLARES SHOULD BE LOCATED OUTSIDE THE DIRECT LINE OF TRAVEL MOST LIKELY TO BE FOLLOWED BY THE VISUALLY IMPAIRED.
- SIGNS, POLES, PLANTERS, MAILBOXES, ETC. SHALL NOT BE LOCATED WHERE THEY WILL INTERFERE WITH THE USE OF SIDEWALK RAMPS. UTILITY POLES MAY BE INCORPORATED INTO THE FLARES OF THE CURB RAMP.
- SIDEWALK RAMPS SHALL NOT BE LOCATED WHERE USERS MUST CROSS DROP INLET GRATES, MANHOLE COVERS, OR OTHER ACCESS LIDS. IF THIS CANNOT BE AVOIDED, THEN GRATE DESIGN AND PLACEMENT SHALL CONFORM TO ADA REQUIREMENTS.
- THE PUBLIC SIDEWALK CURB RAMP STANDARDS DEPICTED HERE MAY NOT BE APPROPRIATE FOR ALL LOCATIONS. FIELD CONDITIONS AT INDIVIDUAL LOCATIONS MAY REQUIRE SPECIFIC DESIGNS. RAMP DESIGNS THAT ARE COMPLIANT WITH ADA BUT NOT LISTED IN THESE DETAILS MAY BE APPROVED BY THE CITY ENGINEER.
- DESIGNS MUST BE CONSISTENT WITH THE PROVISIONS OF THESE DETAILS AND TO ADA GUIDELINES TO THE MAXIMUM EXTENT FEASIBLE ON ALTERATION PROJECTS, AND WHEN STRUCTURALLY PRACTICABLE ON NEW CONSTRUCTION PROJECTS AS REQUIRED BY THE ADA ACCESSIBILITY GUIDELINES.
- MAXIMUMS AND MINIMUMS DO NOT HAVE TOLERANCES AND ARE NOT TO BE EXCEEDED OR UNMET.
- THE STANDARD TURNING SPACE (LEVEL LANDING) IS 4'-0" X 4'-0" SLOPING NO MORE THAN 2.0% WHERE THE TURNING SPACE SHALL BE 4'-0" MINIMUM BY 5'-0" MINIMUM. THE 5'-0" DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
- DETECTABLE WARNINGS SHALL BE INSTALLED AT ALL SIGNED OR SIGNALIZED INTERSECTIONS. THEY SHALL HAVE A TRUNCATED DOME SURFACE. THE DOMES SHALL BE IN A SQUARE PATTERN AND ALIGNED WITH PEDESTRIAN TRAFFIC WHERE POSSIBLE.
- DETECTABLE WARNINGS SHALL SPAN THE WIDTH OF THE PEDESTRIAN RAMP. SEE STANDARD DETAIL FIGURE I-7 (I-7A).
- ALL DETECTABLE WARNING FIELDS PLACED AT THE SAME INTERSECTION SHALL BE MADE UP OF THE SAME UNIFORM MATERIAL TYPE. DETECTABLE WARNINGS SHALL BE CONTRASTING IN COLOR TO THE ADJACENT WALKWAY.
- A RAMP WITH A RUNNING SLOPE LESS THAN 5.0% IS DEFINED AS A "BLENDED TRANSITION". BLENDED TRANSITIONS DO NOT REQUIRE A LEVEL LANDING AT THE TOP OF THE RAMP.
- THE RAMP LENGTH SHALL NOT EXCEED 15 FEET. ADJUST RAMP LENGTH OR SLOPE AS NEEDED TO PROVIDE ACCESS TO THE MAXIMUM EXTENT FEASIBLE.
- ALL CURB RAMP JOINTS AND GRADE BREAKS SHALL BE FLUSH.
- RAMP GRADE BREAKS SHALL BE PERPENDICULAR TO THE RUNNING SLOPE.
- THERE SHALL BE A MINIMUM OF 12" AGGREGATE SUBBASE COURSE - GRAVEL UNDER THE SIDEWALK SURFACE ON PEDESTRIAN RAMPS.
- DRAINAGE STRUCTURES, UTILITY POLES, TRAFFIC SIGNAL EQUIPMENT OR OTHER OBSTRUCTIONS SHALL NOT BE INSTALLED IN THE CURB RAMP OR TURNING SPACE AREAS.
- BEFORE RETROFITTING RAMPS, THE CONTRACTOR SHALL VERIFY REMOVAL LIMITS ARE SUFFICIENT TO PROVIDE POSITIVE DRAINAGE, MAINTAIN EXISTING DRAINAGE PATTERNS AND AVOID PONDING IN THE FINAL CONFIGURATION.
- A TEMPORARY PEDESTRIAN ACCESS ROUTE SHALL BE PROVIDED WHENEVER THE EXISTING PEDESTRIAN ACCESS ROUTE IN THE PUBLIC RIGHT OF WAY IS BLOCKED BY CONSTRUCTION, ALTERATION, MAINTENANCE OR OTHER TEMPORARY CONDITIONS. REFER TO MUTCD FOR GUIDANCE.

NOTES:

- ALL DETECTABLE WARNING PLATES SHALL BE UNCOATED CAST IRON. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
- CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR MDOT CLASS A STRUCTURAL CONCRETE. MINIMUM COMPRESSIVE STRENGTH 4,000 PSI. THE EXPOSED CONCRETE BORDER SHALL RECEIVE A UNIFORM BROOM FINISH PERPENDICULAR TO THE FLOW OF PEDESTRIAN TRAFFIC.
- TRUNCATED DOMES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. TRUNCATED DOME BRICKS AND GRANITE PAVERS ARE NOT ALLOWED.
- SIZE: THE DETECTABLE WARNING PLATES SHALL EXTEND 24 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE FULL WIDTH OF THE CURB RAMP, LANDING, OR BLENDED TRANSITION TO THE STREET.
- ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.



SIDEWALK RAMP DETECTABLE WARNING PANEL
NOT TO SCALE

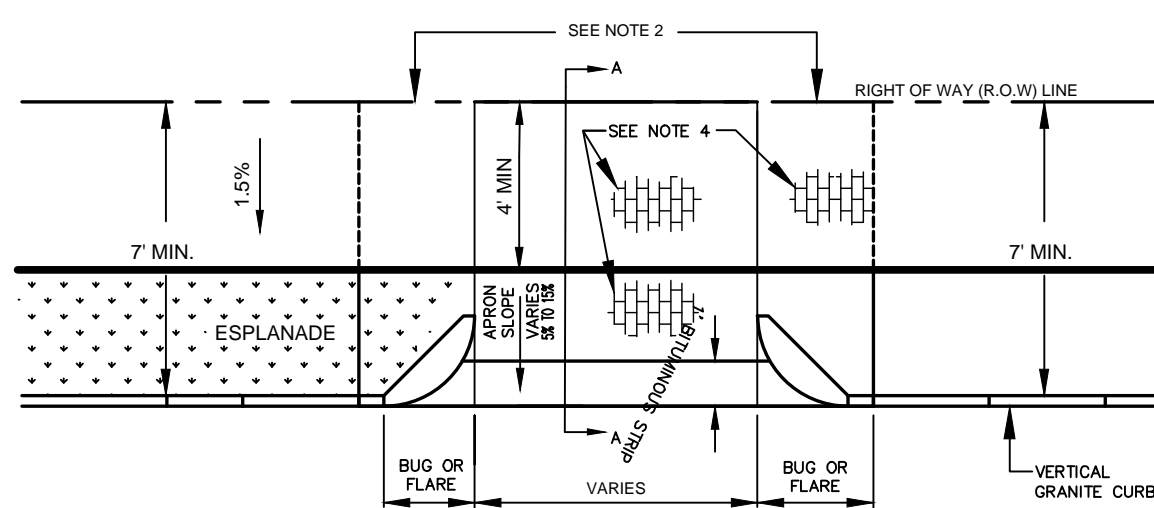
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DESIGNED BY:	E. BOLT
DRAWN BY:	E. BOLT
CHECKED BY:	
SHEET NAME:	DETAILS
DATE:	SEPT 2024
SHEET NO.:	19

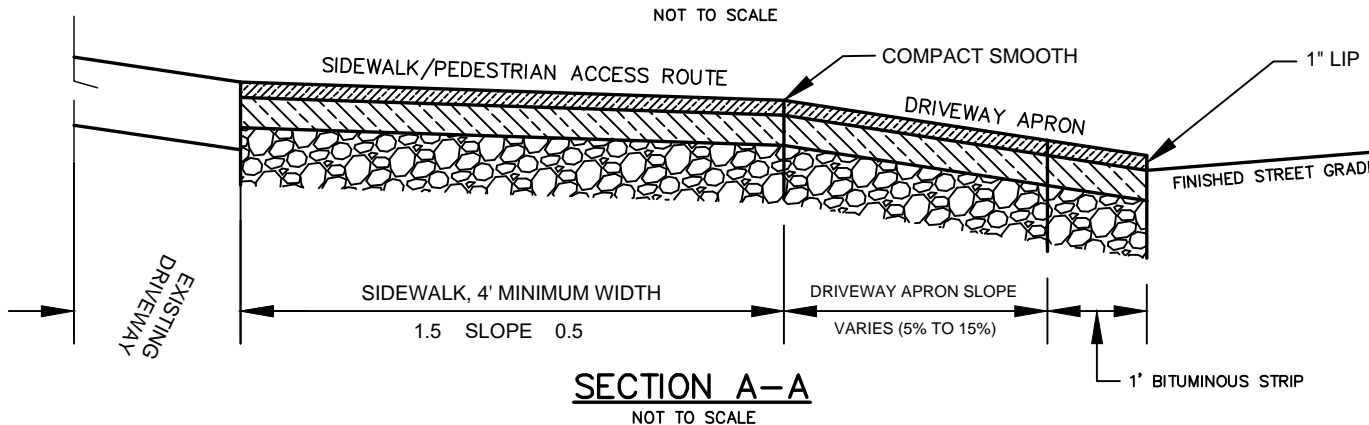
PROJECT NAME:
2025 CDBG SIDEWALK AND ADA IMPROVEMENT PROJECT
CITY OF PORTLAND, MAINE
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
212 CANCO RD. SUITE B, PORTLAND, MAINE 04101
PHONE (207) 874-8801 FAX (207) 874-8852



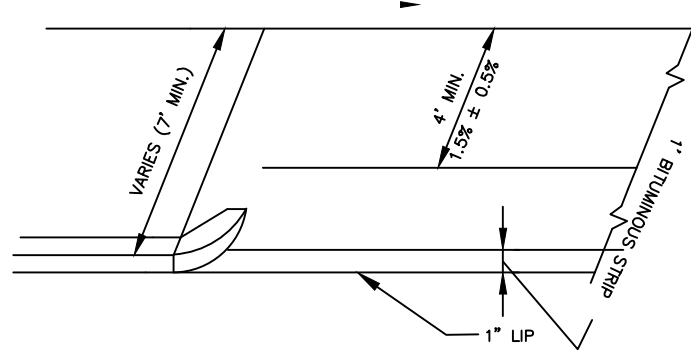
DESIGNED BY:	E. BOLT
DRAWN BY:	E. BOLT
CHECKED BY:	
SHEET NAME:	DETAILS
DATE:	SEPT 2024
SHEET NO.:	19



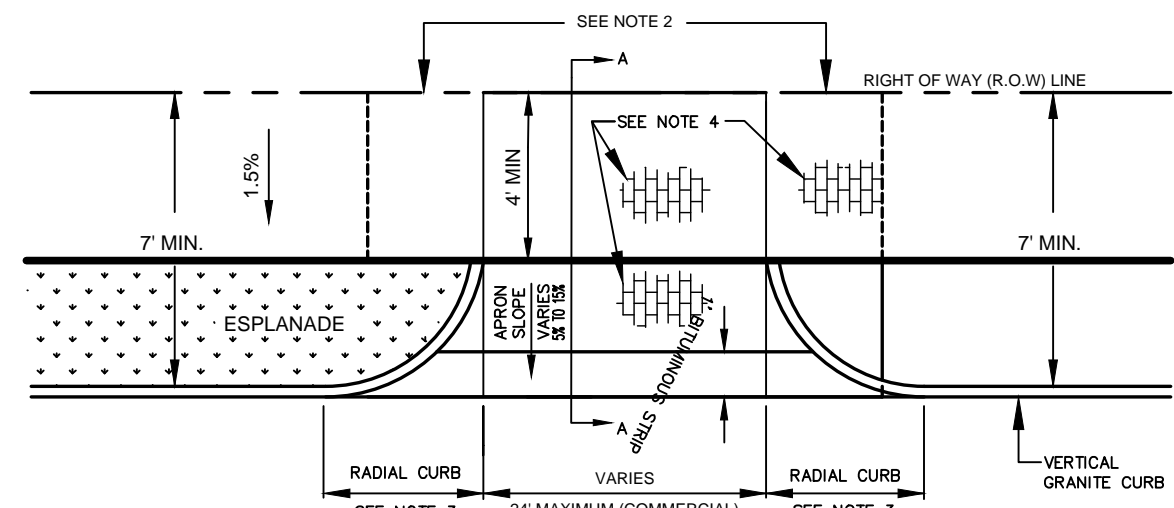
STANDARD RESIDENTIAL DRIVEWAY APRON LAYOUT
NOT TO SCALE



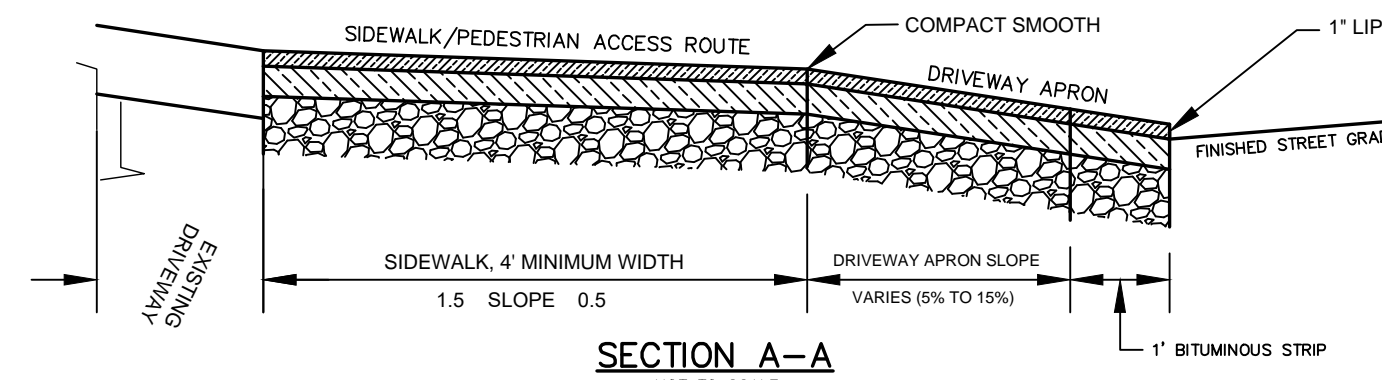
SECTION A-A
NOT TO SCALE



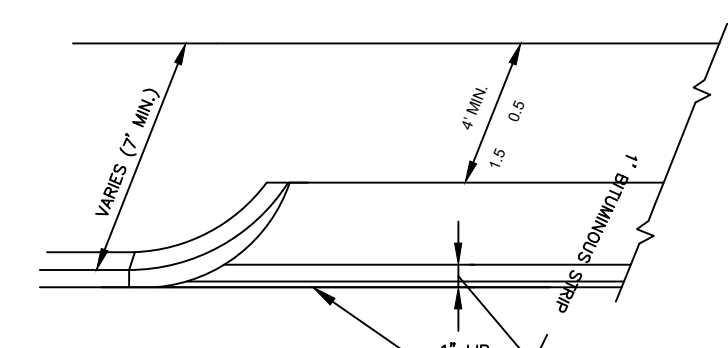
- NOTES:
- MATCH EXISTING GRADE AT R.O.W. LINE. WHERE REQUIRED TO MEET A.D.A. AND DRIVEWAY APRON SLOPE REQUIREMENTS, A CONSTRUCTION EASEMENT MAY BE REQUIRED.
 - IF MATCHING THE EXISTING GRADE AT ROW CAUSES APRON SLOPE TO EXCEED RECOMMENDED %, SIDEWALK TRANSITION AREAS MAY BE ADDED, BUT SHALL NOT EXCEED 8.33%.
 - TWO-FOOT RADIUS CORNER GRANITE PIECES ('BUGS') ARE REQUIRED AT LOW-VOLUME DRIVEWAYS UNLESS OTHERWISE DIRECTED BY DPW. AT OTHER DRIVEWAYS, STANDARD FLARE/TIPODOWN TREATMENTS SHALL BE USED.
 - ALL MATERIALS IN ROW TO COMPLY WITH CITY'S SIDEWALK & DRIVEWAY MATERIAL POLICY. BRICK IN DRIVEWAY APRONS, WHERE APPLICABLE, SHALL BE ORIENTED IN THE SAME WAY AS THE ADJACENT BRICK SIDEWALK.
 - THIS LAYOUT MAY BE REQUIRED AT MINOR DRIVEWAYS SERVING COMMERCIAL OR INDUSTRIAL USES.



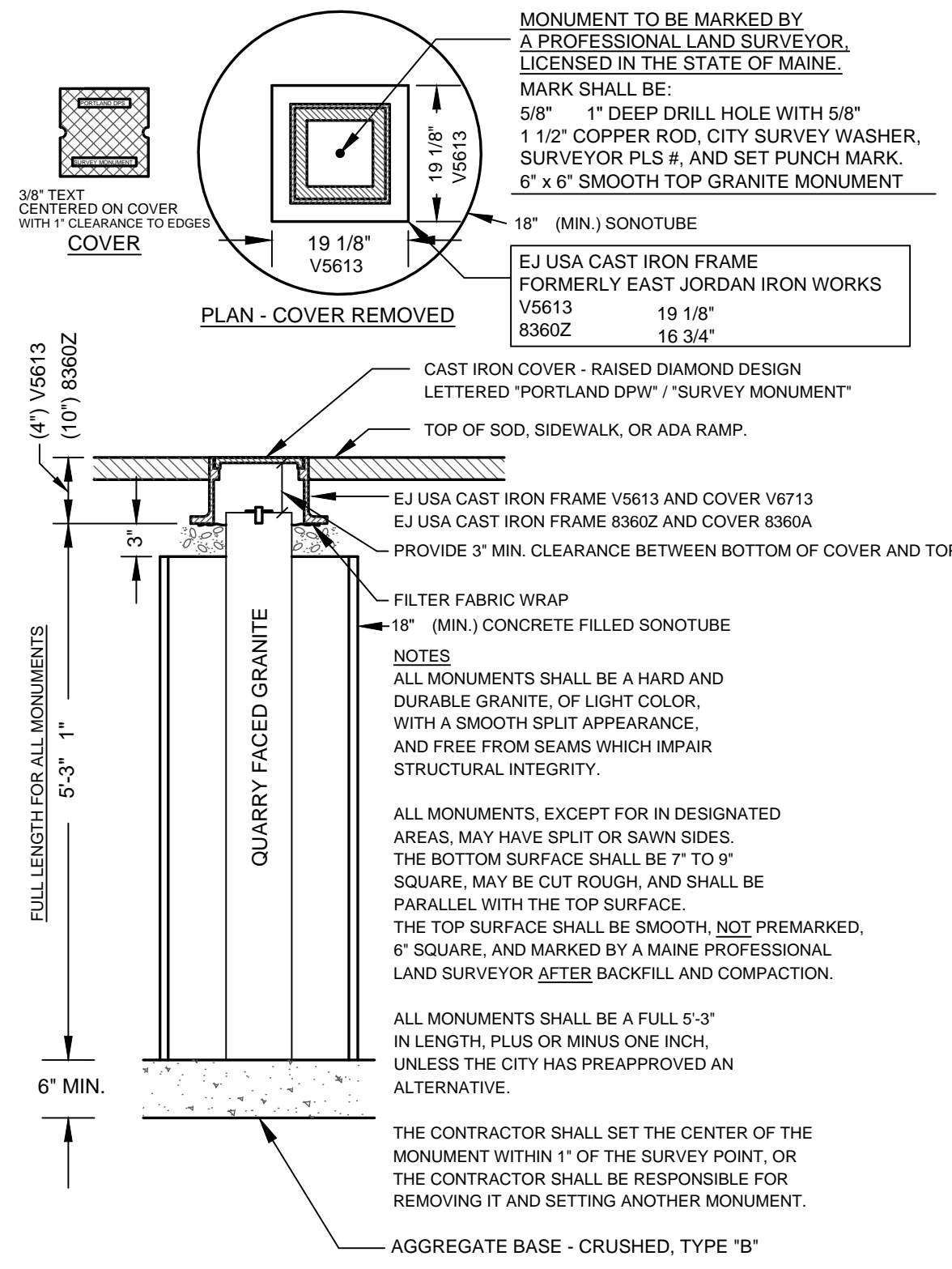
STANDARD COMMERCIAL DRIVEWAY APRON LAYOUT
NOT TO SCALE



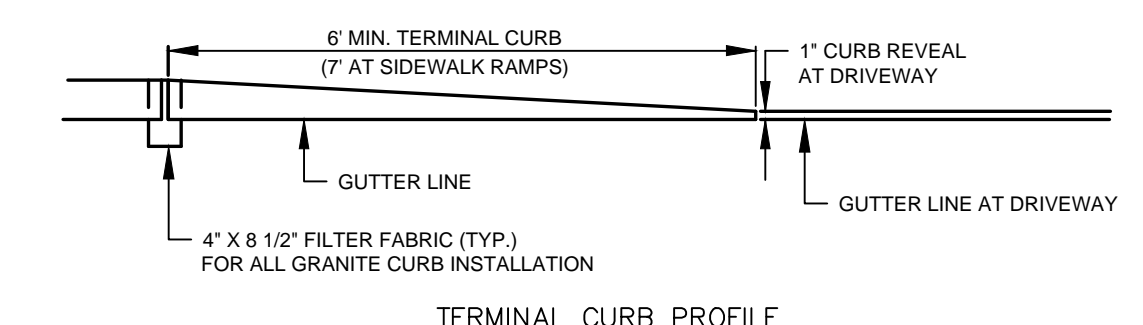
SECTION A-A
NOT TO SCALE



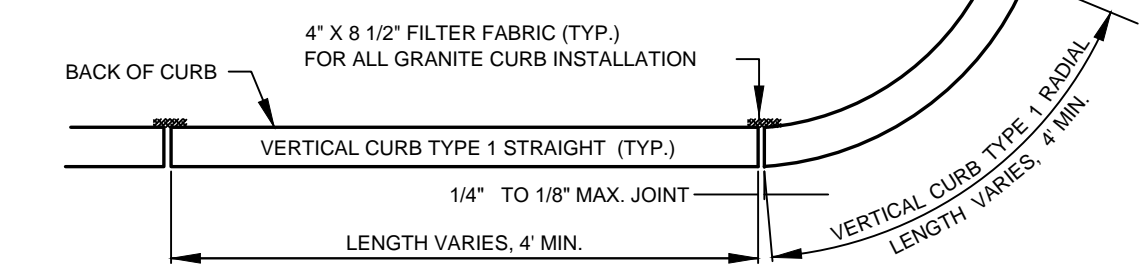
- NOTES:
- MATCH EXISTING GRADE AT R.O.W. LINE, WHERE REQUIRED TO MEET A.D.A. AND DRIVEWAY APRON SLOPE REQUIREMENTS, A CONSTRUCTION EASEMENT MAY BE REQUIRED.
 - IF MATCHING THE EXISTING GRADE AT ROW CAUSES APRON SLOPE TO EXCEED RECOMMENDED %, SIDEWALK TRANSITION AREAS MAY BE ADDED, BUT SHALL NOT EXCEED 8.33%.
 - DRIVEWAY CURB RADIUS SHALL NOT BE LARGER THAN REQUIRED BY THE SITE'S DESIGN VEHICLE. BRICK IN DRIVEWAY APRONS, WHERE APPLICABLE, SHALL BE ORIENTED IN THE SAME WAY AS THE ADJACENT BRICK SIDEWALK.
 - ALL MATERIALS IN ROW TO COMPLY WITH CITY'S SIDEWALK & DRIVEWAY MATERIAL POLICY. BRICK IN DRIVEWAY APRONS, WHERE APPLICABLE, SHALL BE ORIENTED IN THE SAME WAY AS THE ADJACENT BRICK SIDEWALK.



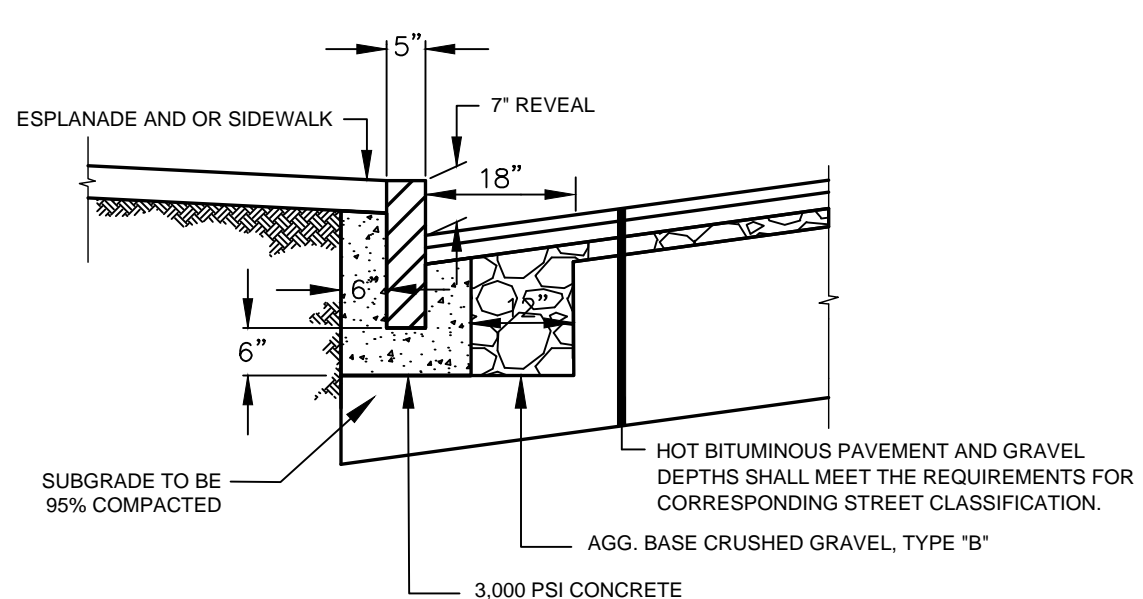
GRANITE STREET MONUMENT
NOT TO SCALE



TERMINAL CURB PROFILE



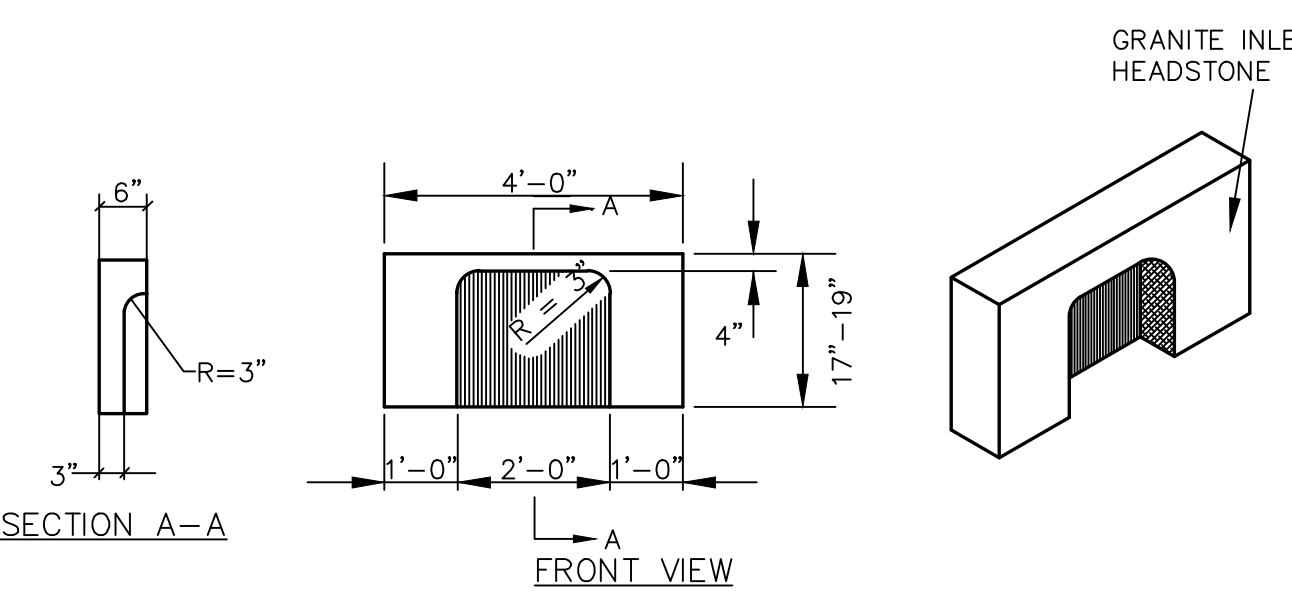
VERTICAL GRANITE CURB PLAN VIEW



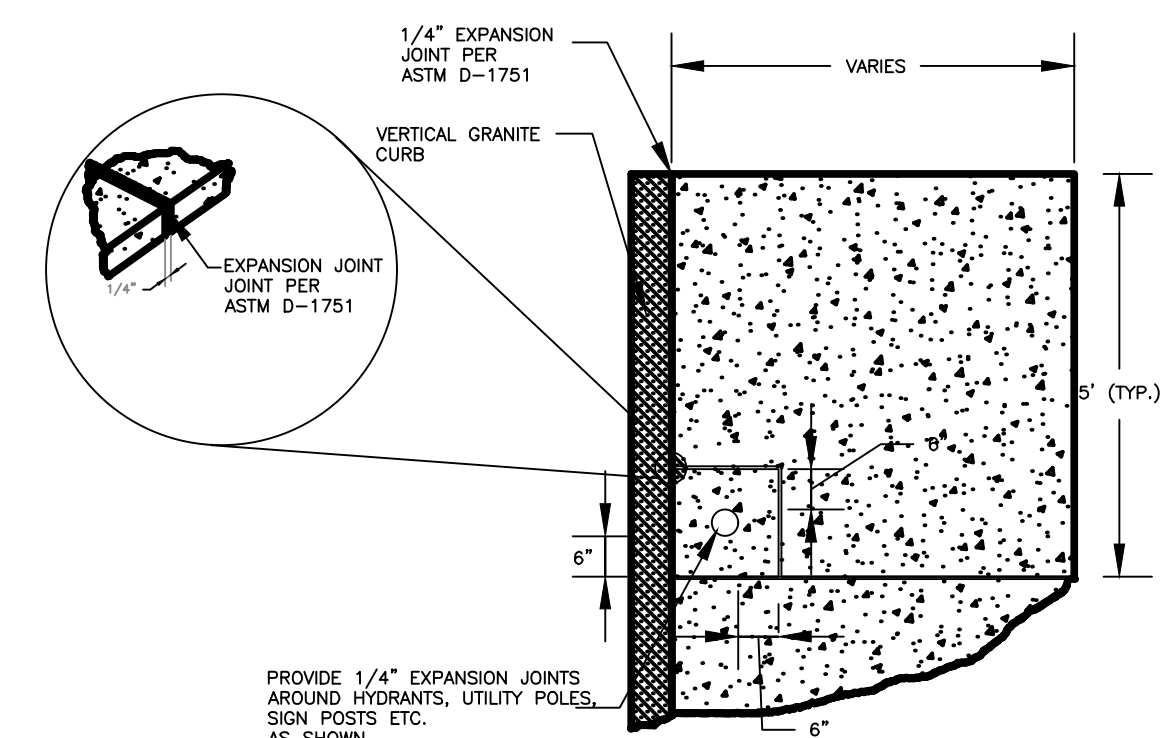
VERTICAL GRANITE CURB CROSS SECTION

- NOTES:
- INDIVIDUAL PIECES OF CURB SHORTER THAN 4' L.F. ARE NOT ALLOWED, WITH THE EXCEPTION OF RADIAL CURB.
 - THE MINIMUM HEIGHT OF THE CONCRETE BACKING SHALL BE 10-INCHES OR TO THE BOTTOM OF THE HOT MIX ASPHALT PAVEMENT, WHICHEVER IS GREATER, MEASURED FROM THE BOTTOM OF CURB.
 - CURBING SHALL BE SET IN A 6-INCH BED OF POURED IN PLACE 3,000 PSI CONCRETE MIX.
 - WOODEN SHIMS MAY BE USED TO SET CURB.

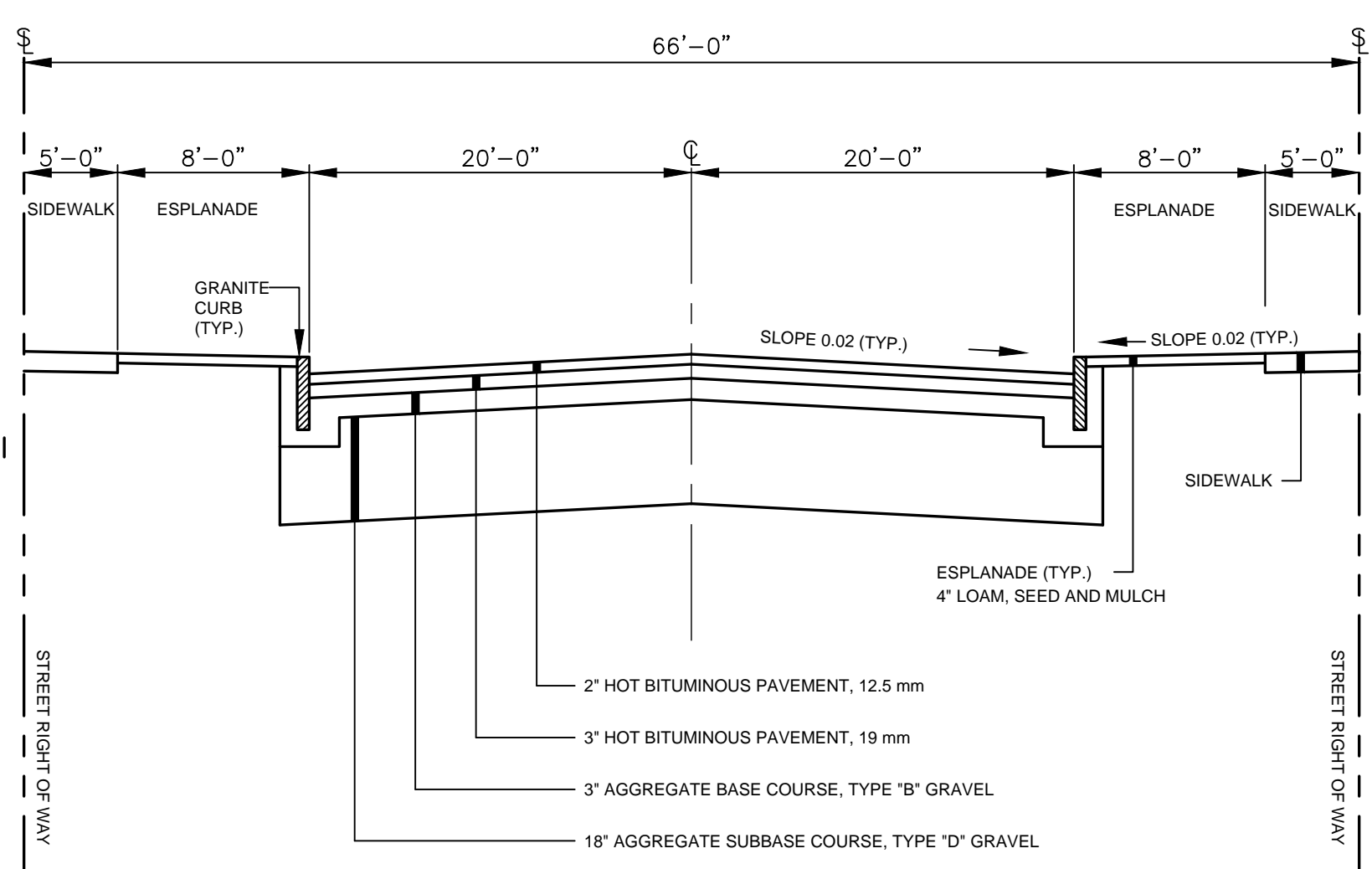
VERTICAL GRANITE CURB
NOT TO SCALE



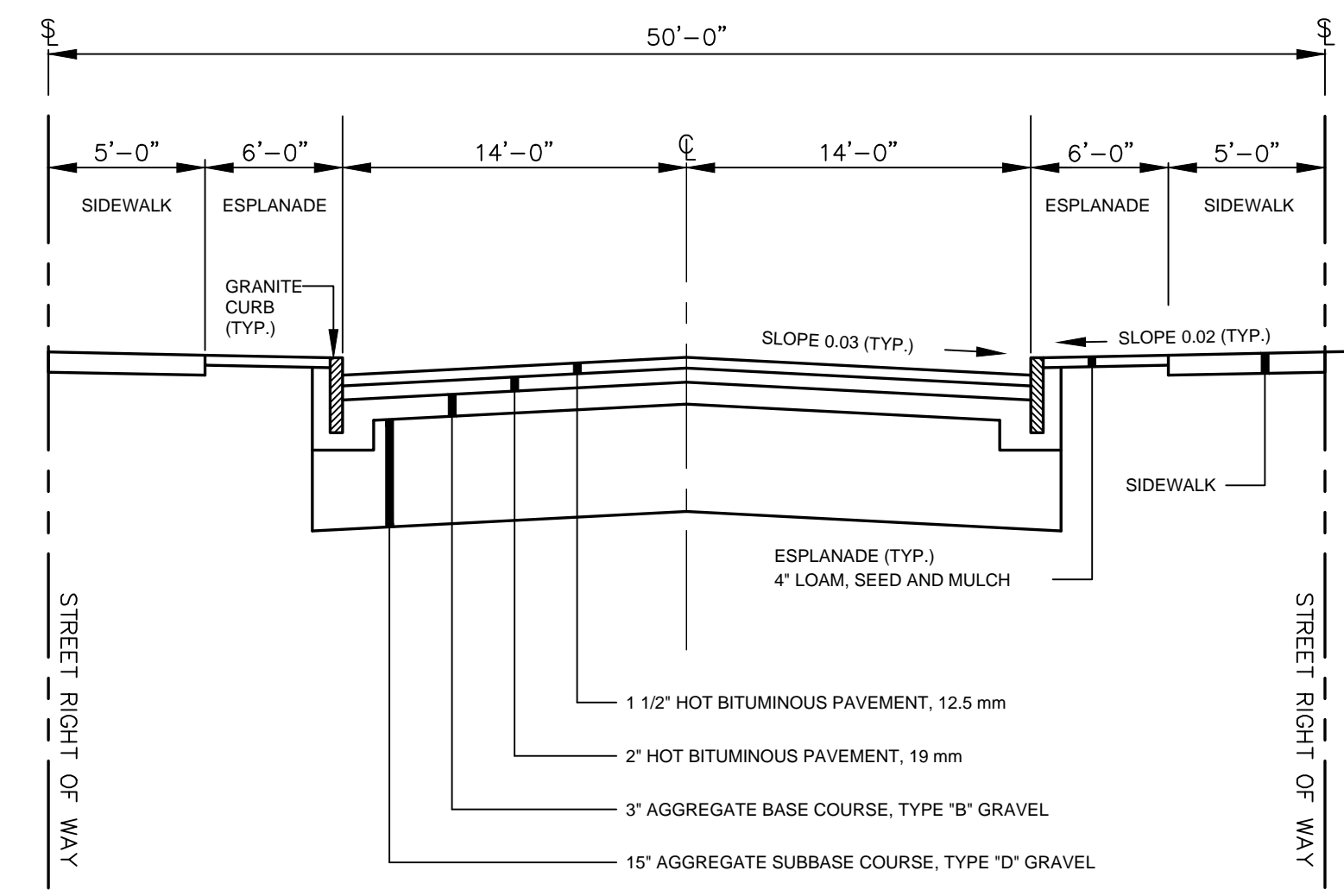
4' GRANITE HEADSTONE FOR CATCH BASIN INLET
NOT TO SCALE



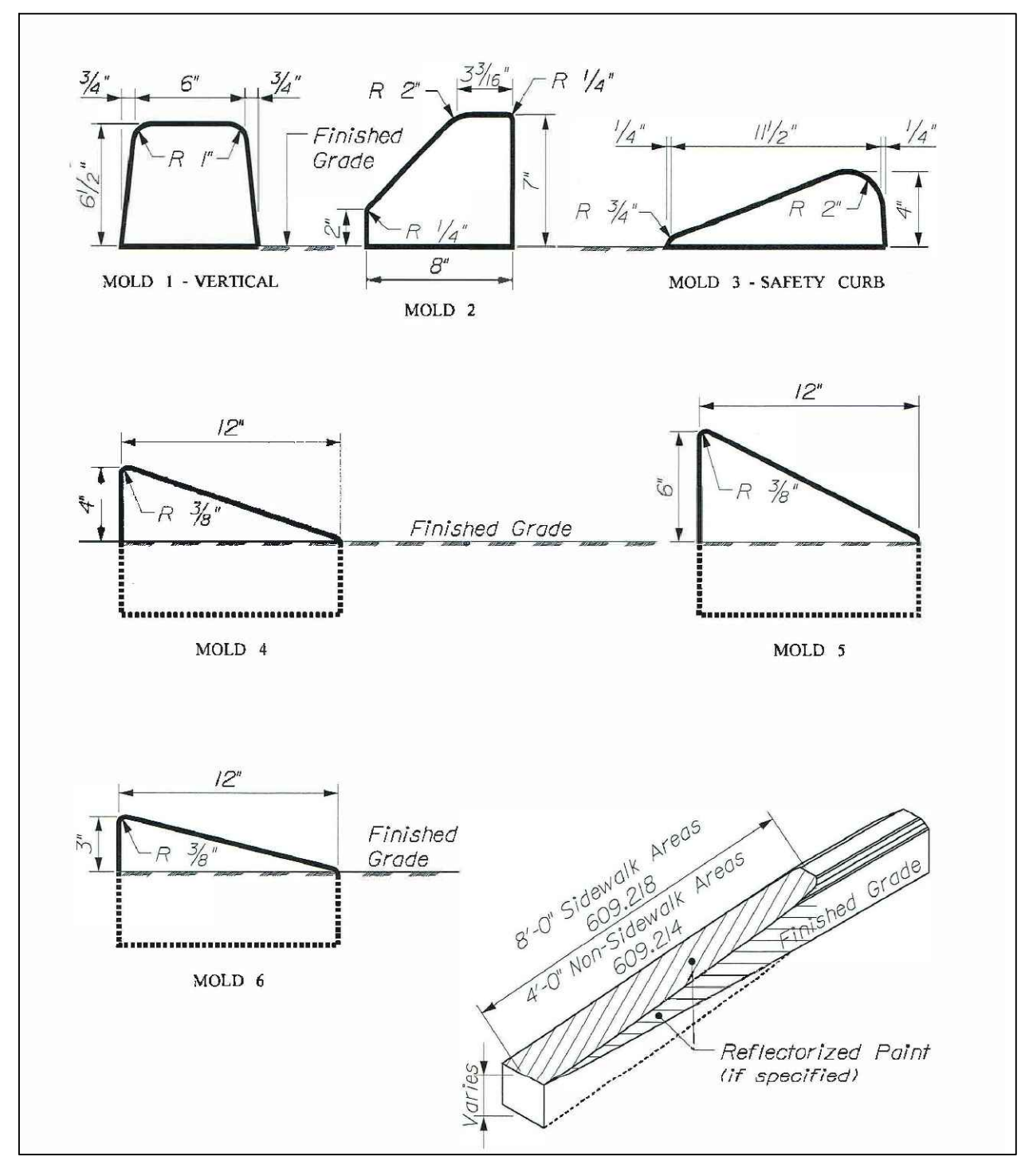
SIDEWALK EXPANSION JOINT
NOT TO SCALE



ARTERIAL STREET CROSS SECTION
NOT TO SCALE



LOCAL STREET CROSS SECTION
NOT TO SCALE



SLIPFORM CONCRETE CURB
NOT TO SCALE

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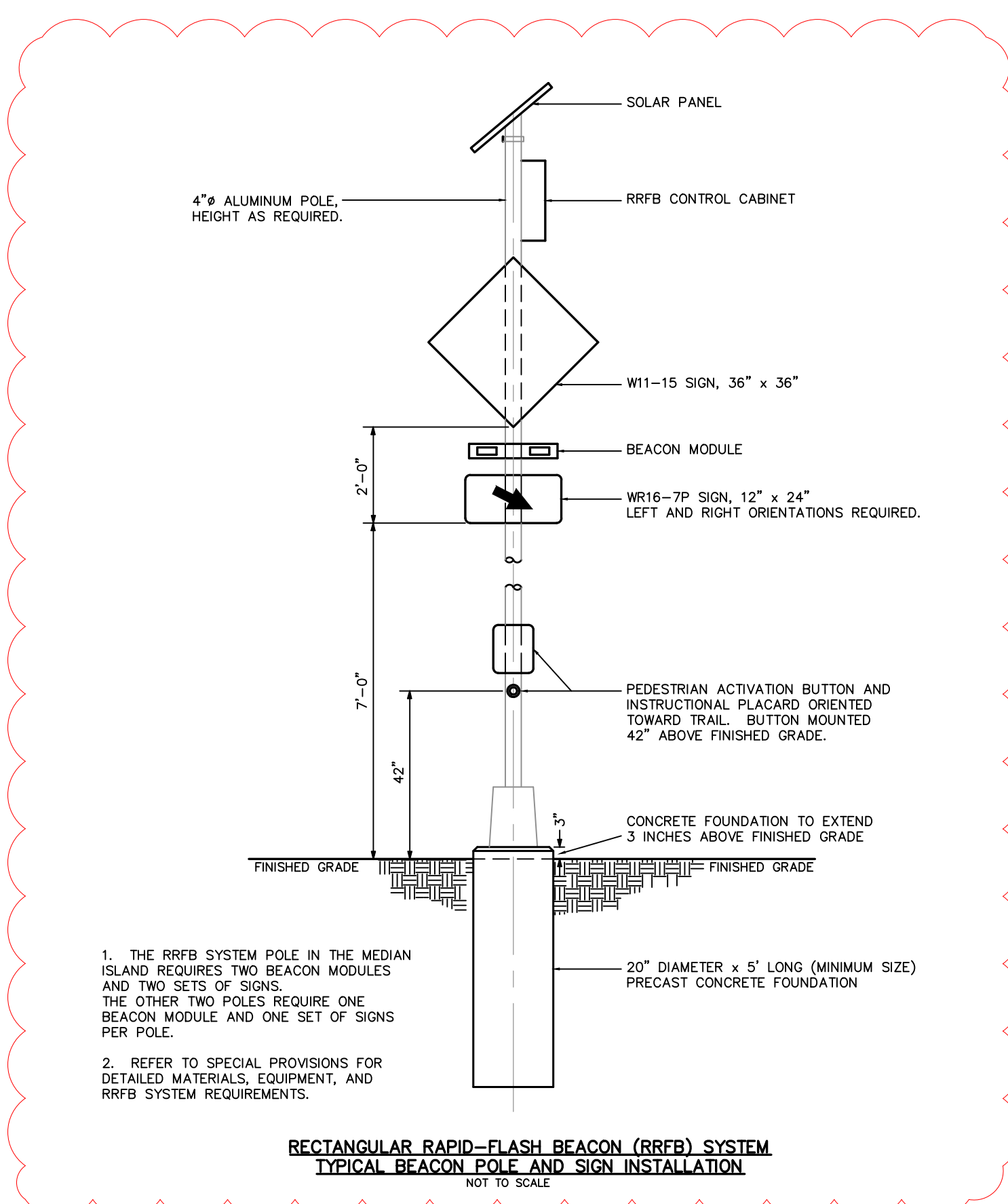
DESIGN BY:	E. BOLT
DRAWN BY:	E. BOLT
CHECKED BY:	L.K. SCELZA
SCALE:	N.E. ENGINEER
AS NOTED	
DATE:	FEB. 28, 2019
PROJECT NAME:	2025 CDBG SIDEWALK AND ADA IMPROVEMENT PROJECT
PROJECT NUMBER:	
DATE:	
REV:	SUBMISSION

2025 CDBG SIDEWALK AND ADA IMPROVEMENT PROJECT

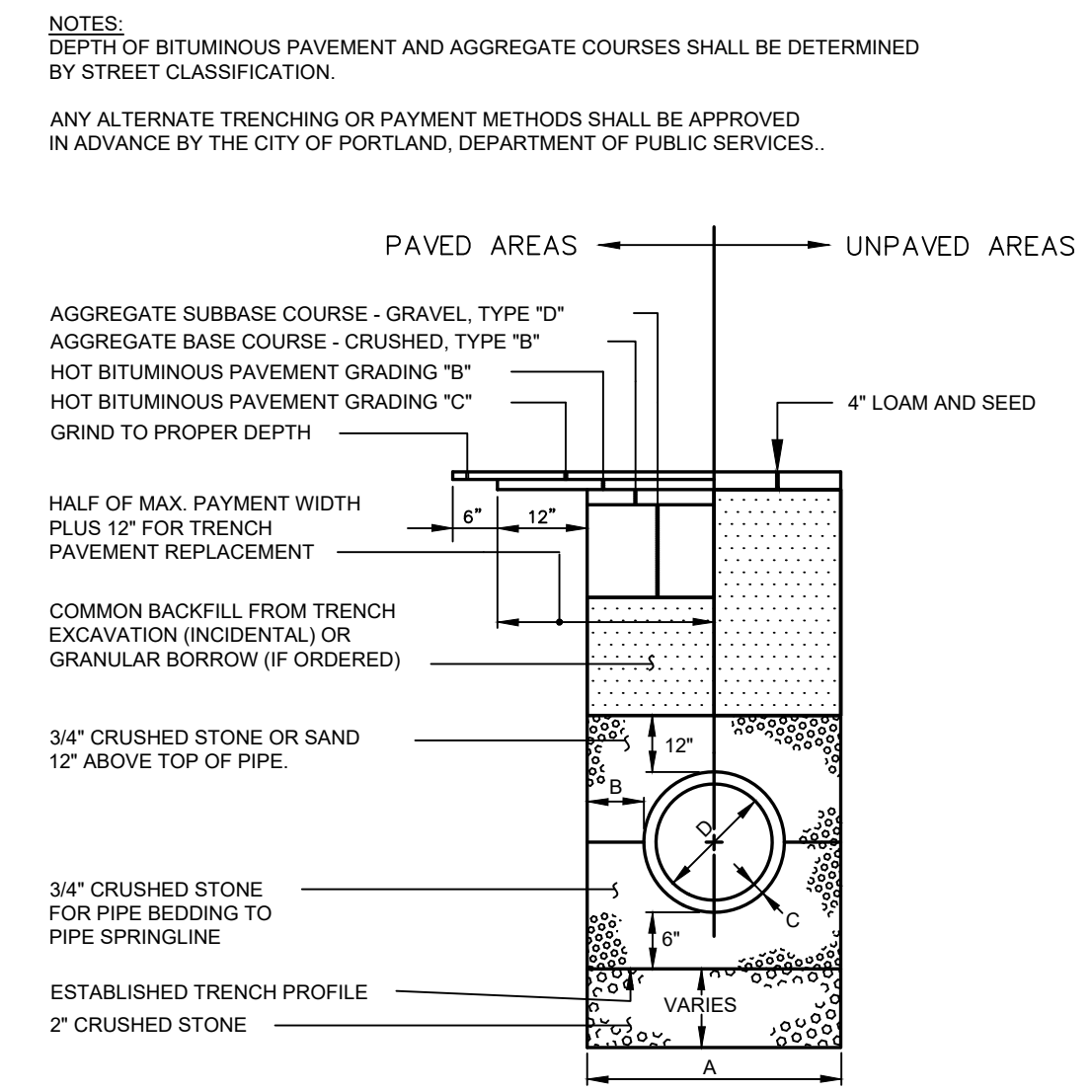
CITY OF PORTLAND, MAINE
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
212 CANCO RD., SUITE B, PORTLAND, MAINE 04101
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DESIGN BY:	E. BOLT
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AS NOTED	
DATE:	SEPT 2024
SHEET NO.:	20



**RECTANGULAR RAPID-FLASH BEACON (RRFB) SYSTEM
TYPICAL BEACON POLE AND SIGN INSTALLATION**
NOT TO SCALE



TYPICAL PIPE TRENCH INSTALLATION
NOT TO SCALE

NOTES

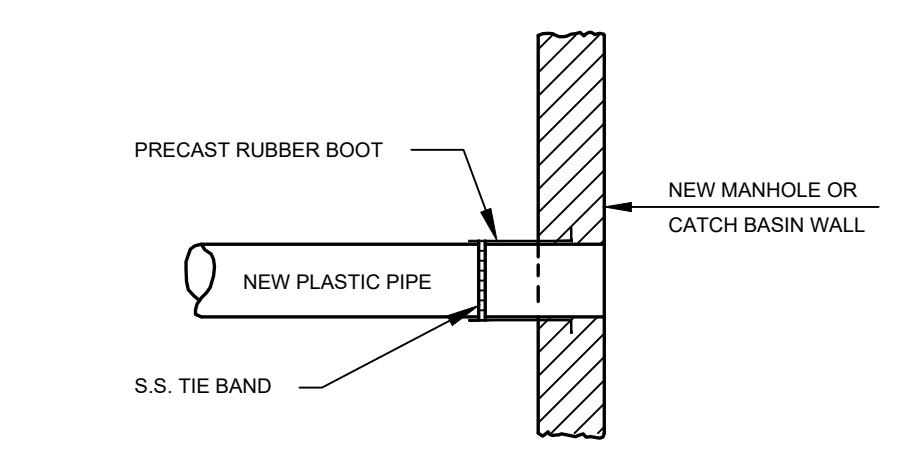
- ALTERNATIVE CONSTRUCTION METHODS OR PAYMENT METHODS SHALL BE APPROVED IN ADVANCE BY THE CITY.
- IN PAVED AREAS, DEPTHS OF GRAVEL AND HOT MIX ASPHALT PAVEMENT SHALL MATCH THE GREATER OF EXISTING CONDITIONS OR THE REQUIREMENTS FOR THE CORRESPONDING STREET CLASSIFICATION.
- DIMENSION B SHALL BE SUFFICIENT TO ALLOW CRUSHED STONE BEDDING TO BE PLACED AND COMPACTED UNDER THE HAUNCHES OF THE PIPE, BUT IN ALL CASES DIMENSION B SHALL BE AT LEAST 9".
- DIMENSION A IS THE MAXIMUM WIDTH ALLOWED FOR CALCULATING PAY QUANTITIES UNDER GRANULAR BORROW, CRUSHED STONE, STRUCTURAL EARTH EXCAVATION, AND STRUCTURAL ROCK EXCAVATION. DIMENSION A SHALL BE BASED ON PIPE DIAMETER D, AS SET FORTH IN THE FOLLOWING TABLE.

PIPE DIAMETER, D (INCHES)	MAX. TRENCH WIDTH, A (FEET)
4	4.0
6	4.0
8	4.0
10	5.0
12	5.0
15	5.0
18	5.0
21	5.0
24	6.0
27	6.0
30	6.0
36	6.0
42	7.0
48	7.0

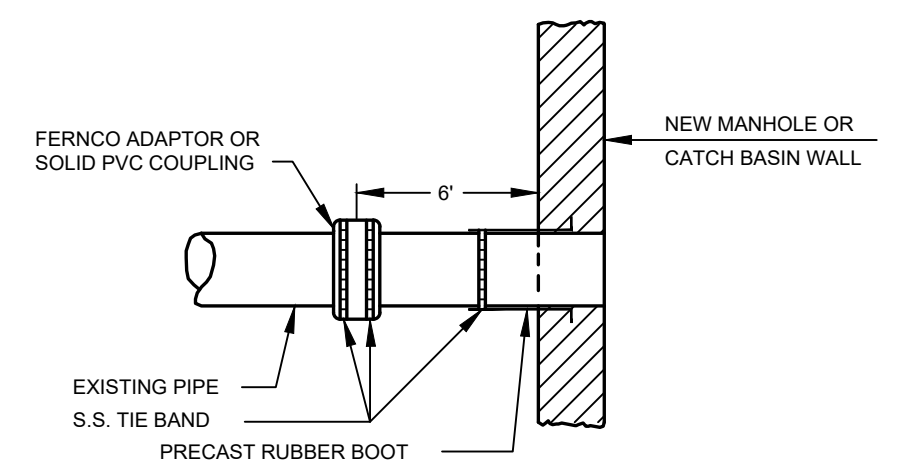
TYPICAL PIPE INSTALLATION - NOTES
NOT TO SCALE

- ALL CONCRETE SHALL HAVE A MINIMUM ULTIMATE COMPRESSIVE STRENGTH OF 4000 LBS. PER SQ. INCH AT THE END OF 28 DAYS, UNLESS OTHERWISE NOTED.
- MANHOLES MAY BE CONSTRUCTED OF PRECAST REINFORCED CONCRETE, OR CAST IN PLACE.
- PRECAST REINFORCED CONE BARREL MANUFACTURED PER ASTM SPEC. C-478.
- ALL STORM AND SEWER MANHOLE COVERS SHALL BE SOLID.
- ALL SANITARY MANHOLE COVERS SHALL HAVE "SEWER" CAST INTO THE COVER. ALL STORMWATER/DRAIN MANHOLE COVERS SHALL HAVE "DRAIN" CAST INTO THE COVER.
- SEWER BRICK SHALL CONFORM TO ASTM SPEC. DESIGNATE ON C-32-63, GRADE MA AND SA.
- ALL SANITARY MANHOLES SHALL HAVE A WATERPROOFING COATING APPLIED TO THE EXTERIOR SURFACE.
- CASTINGS SHALL CONFORM TO ASTM DESIGNATION A48-CLASS 35.
- EXISTING MANHOLES, CATCH BASINS, FRAMES, AND COVERS SHALL BE SALVAGED BY THE CONTRACTOR, AND SHALL REMAIN THE PROPERTY OF THE CITY OF PORTLAND.

GENERAL NOTES FOR MANHOLES AND CATCH BASINS



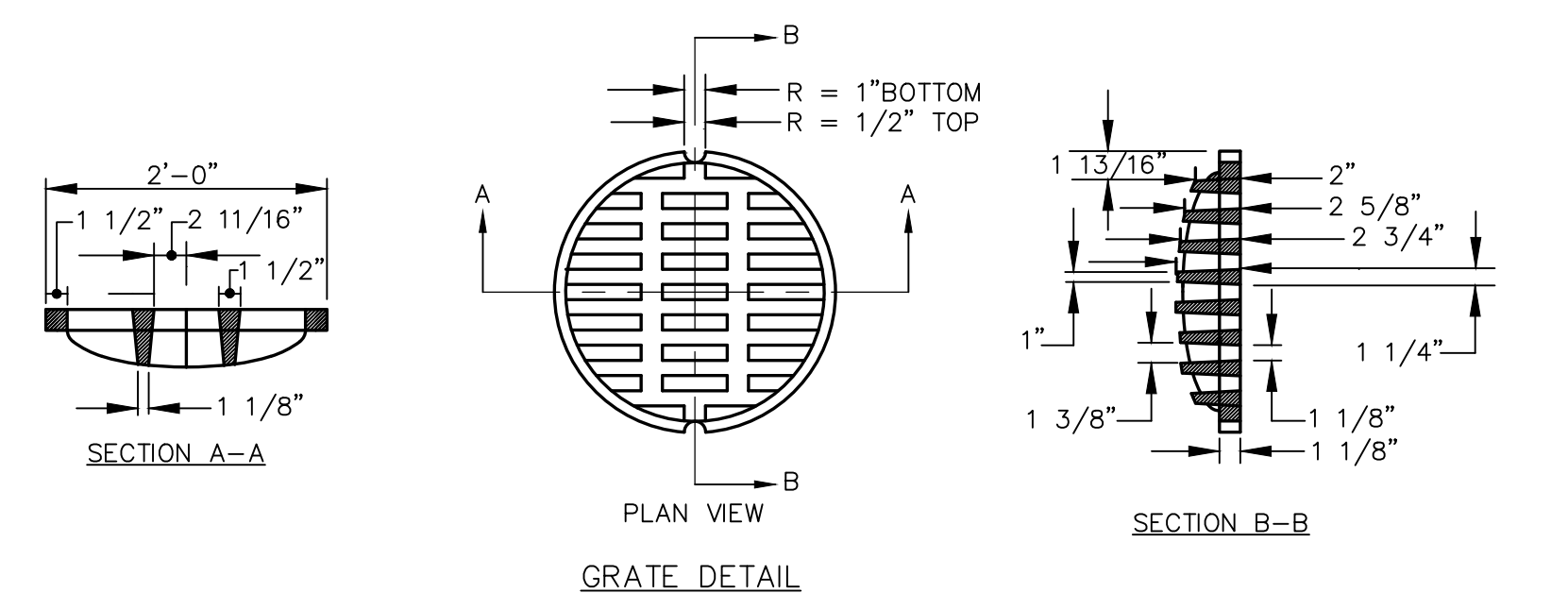
METHOD 2 - NEW CONSTRUCTION



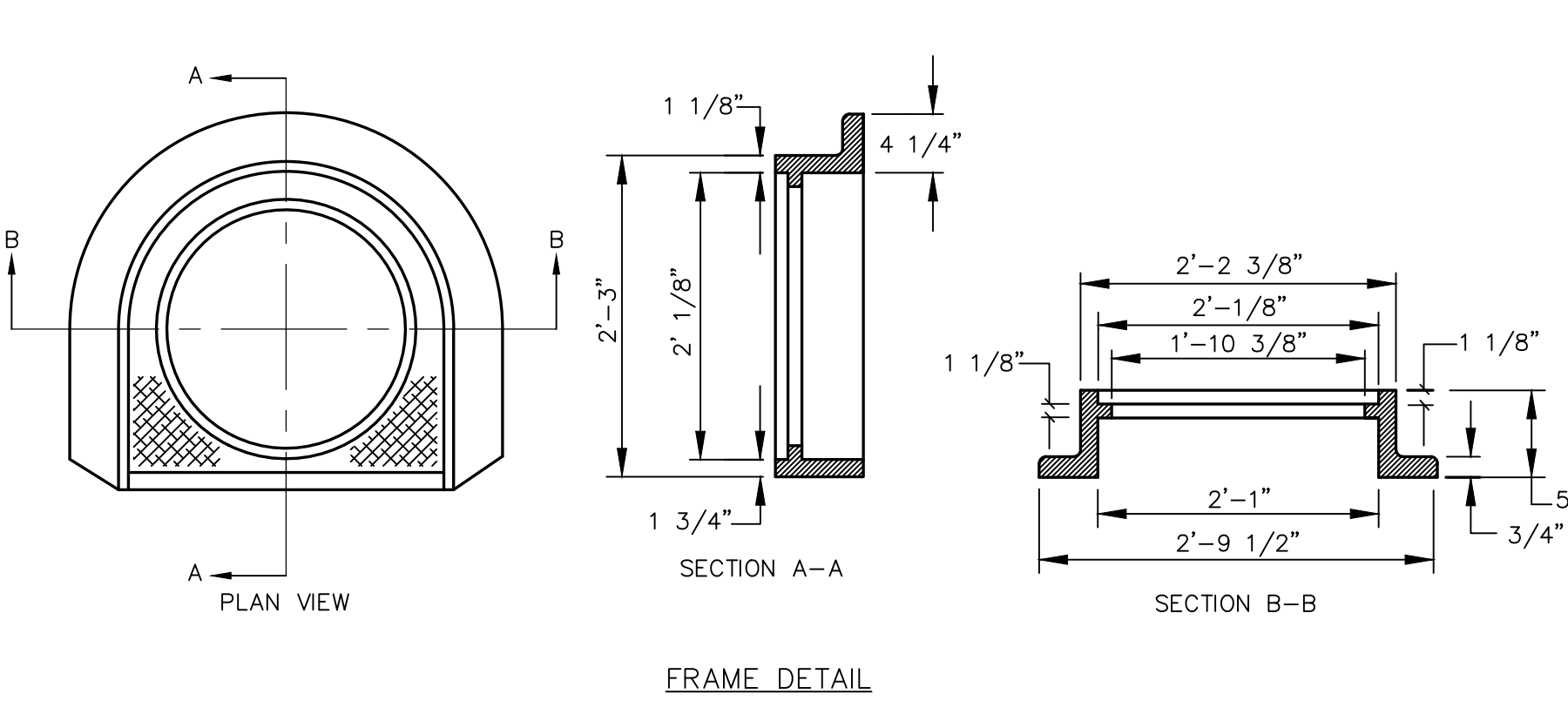
METHOD 1 - EXISTING PIPE INTO NEW STRUCTURE

PLASTIC PIPE CONNECTIONS
NOT TO SCALE

- APPROVED CATCH BASIN FRAMES:
 - EAST JORDAN = 7375Z
 - NEENAH = R-3248
 - OR APPROVED EQUAL
- APPROVED CATCH BASIN GRATES:
 - EAST JORDAN = 2440M
 - NEENAH = R-3248
 - OR APPROVED EQUAL

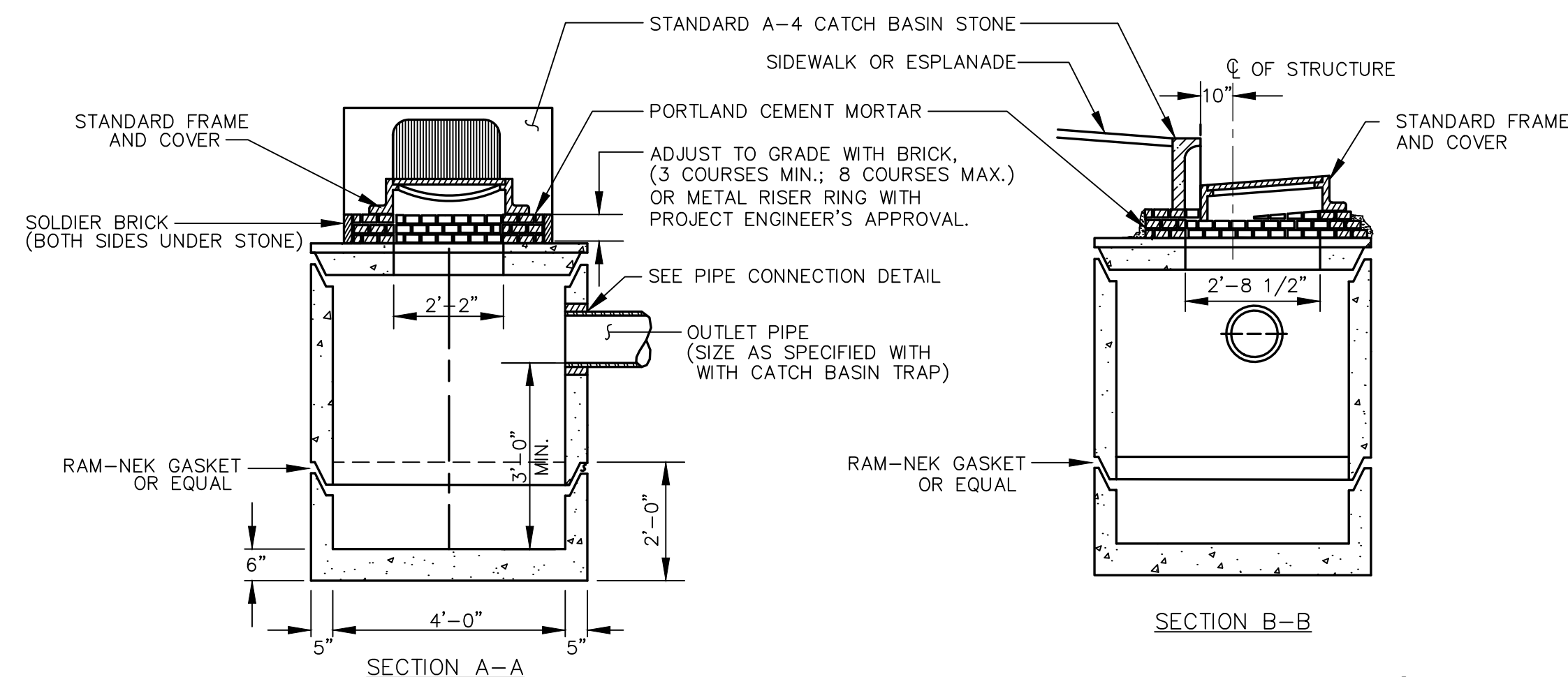


GRATE DETAIL



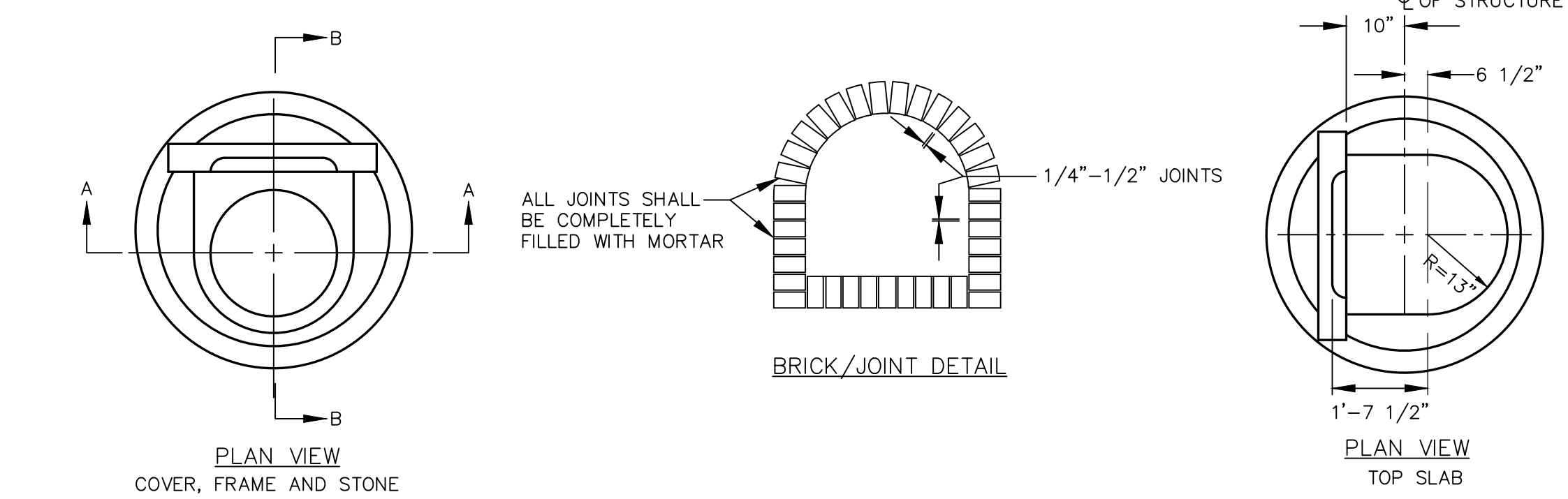
FRAME DETAIL

CATCH BASIN FRAME & GRATE



SECTION A-A

SECTION B-B

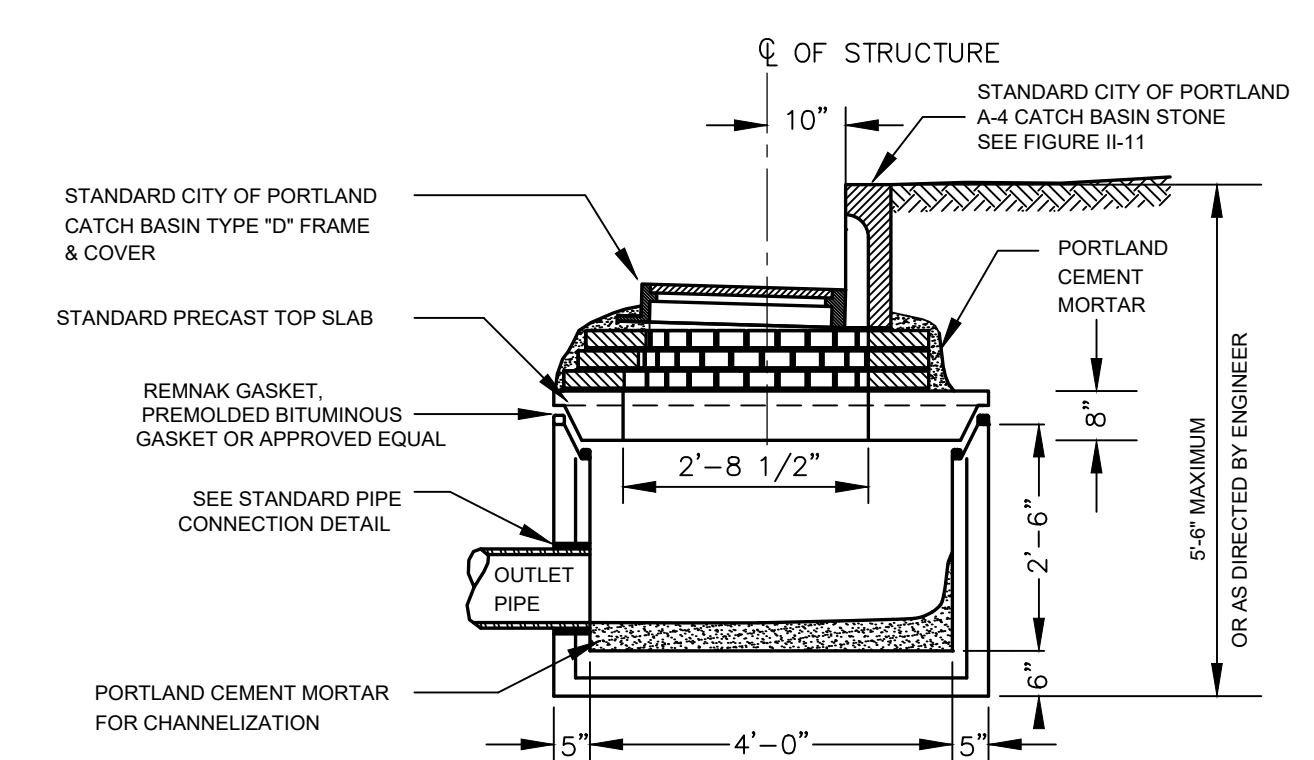


**PLAN VIEW
COVER, FRAME AND STONE**

BRICK/JOINT DETAIL

**PLAN VIEW
TOP SLAB**

PRECAST CONCRETE CATCH BASIN - TYPE "E"
NOT TO SCALE



CATCH BASIN INLET
NOT TO SCALE

SECRET/OPEN

DRAWN BY:	L.K. SCELZA
CHECKED BY:	N.E. ENGINEER
SCALE:	AS NOTED
DATE:	FEB. 28, 2019

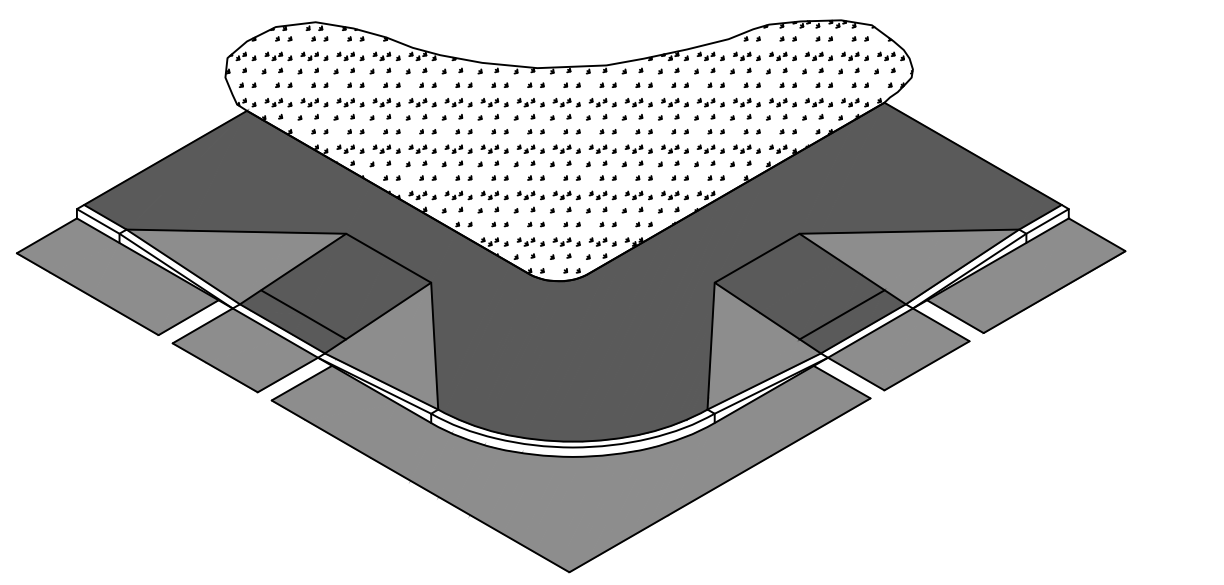
1/25	DATE
DETAILS ADDED	SUBMISSION
1	REV

2024 CDBG SIDEWALK AND ADA IMPROVEMENT PROJECT
CITY OF PORTLAND, MAINE
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
212 CANCO RD., SUITE B, PORTLAND, MAINE 04101
PHONE (207) 874-3801 FAX (207) 874-8852

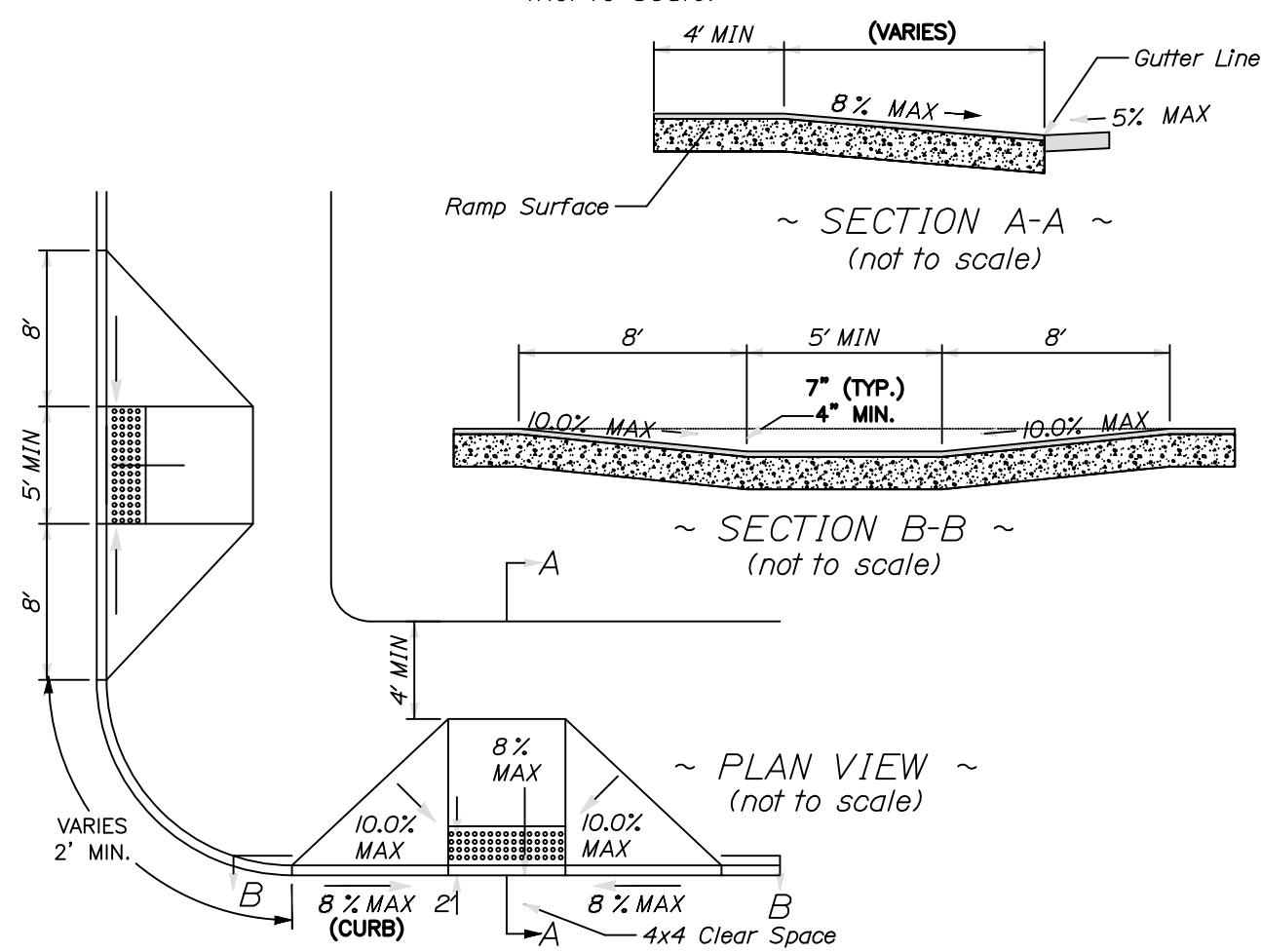


E. BOLT	REV:
E. BOLT	SEPT 2024
E. BOLT	21

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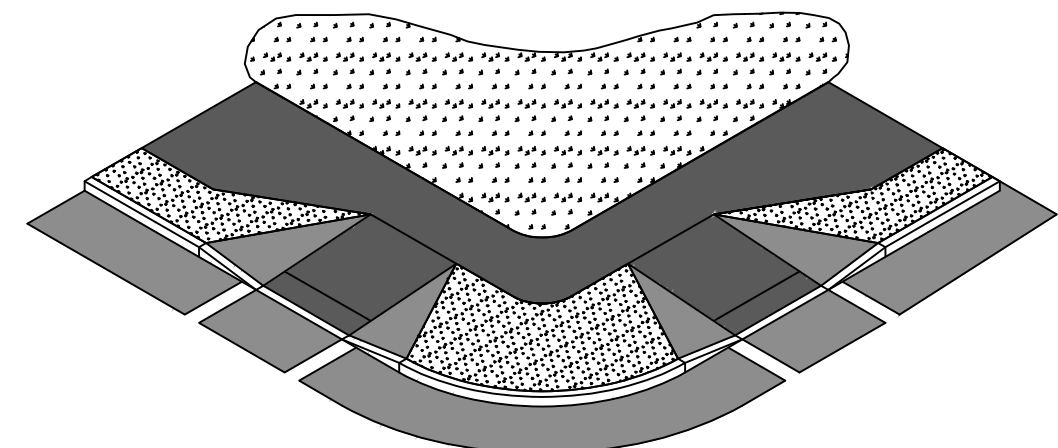


~ ISOMETRIC VIEW ~
(not to scale)

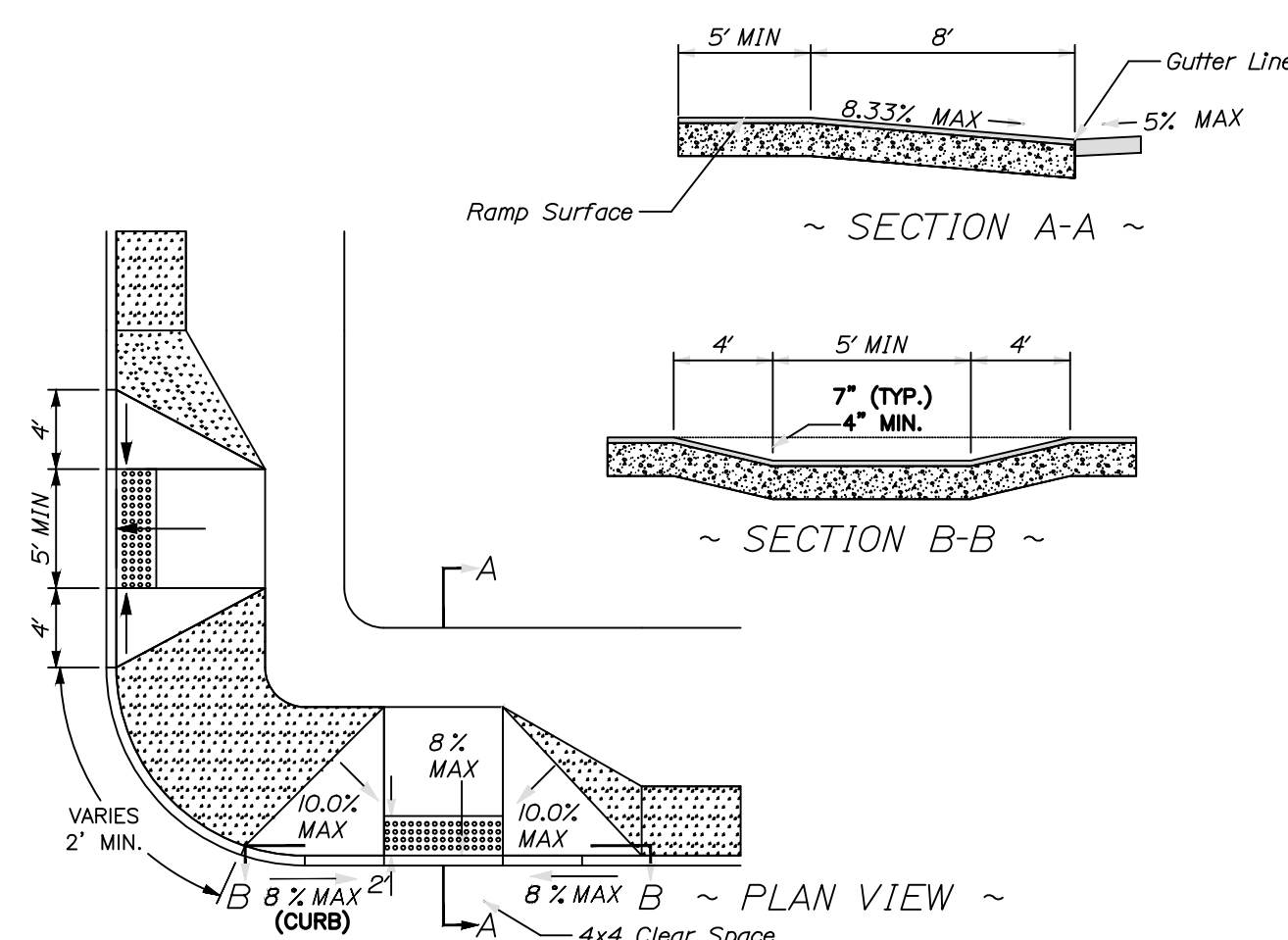


- NOTES:
1. THIS DESIGN IS THE PREFERRED OPTION WHEN THE SIDEWALK WIDTH IS GREATER THAN 6.5'. USE OTHER OPTIONS ONLY WHEN REQUIRED BY DESIGN CONSTRAINTS.
 2. A MINIMUM CURB REVEAL OF 4" IS REQUIRED AT THE APEX OF THE CURB RADIUS.
 3. MINIMUM TERMINAL CURB LENGTH SHALL BE 4'.
 4. SIDEWALK MATERIAL PER CITY SIDEWALK MATERIAL POLICY.

PERPENDICULAR PEDESTRIAN RAMP — SIDEWALK WIDTH 6.5' AND GREATER
NO ESPLANADE
NOT TO SCALE

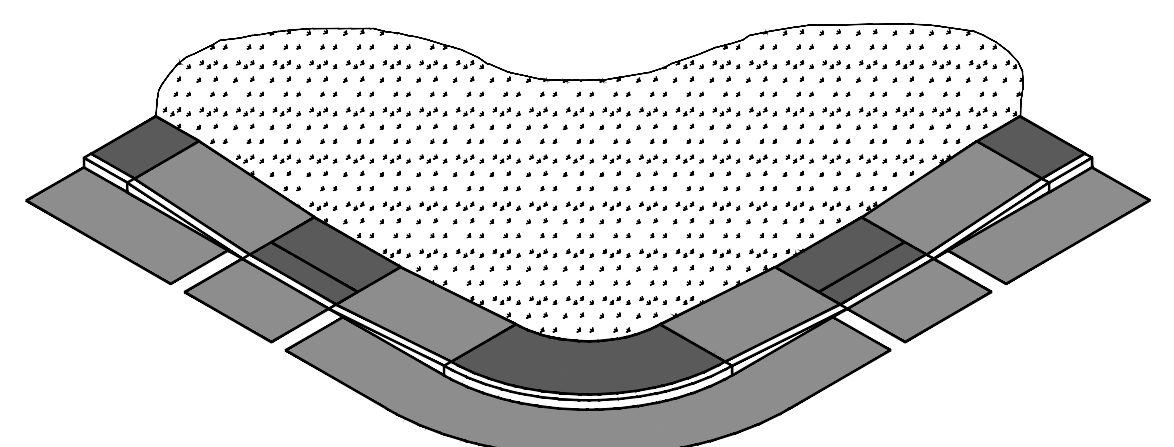


~ ISOMETRIC VIEW ~

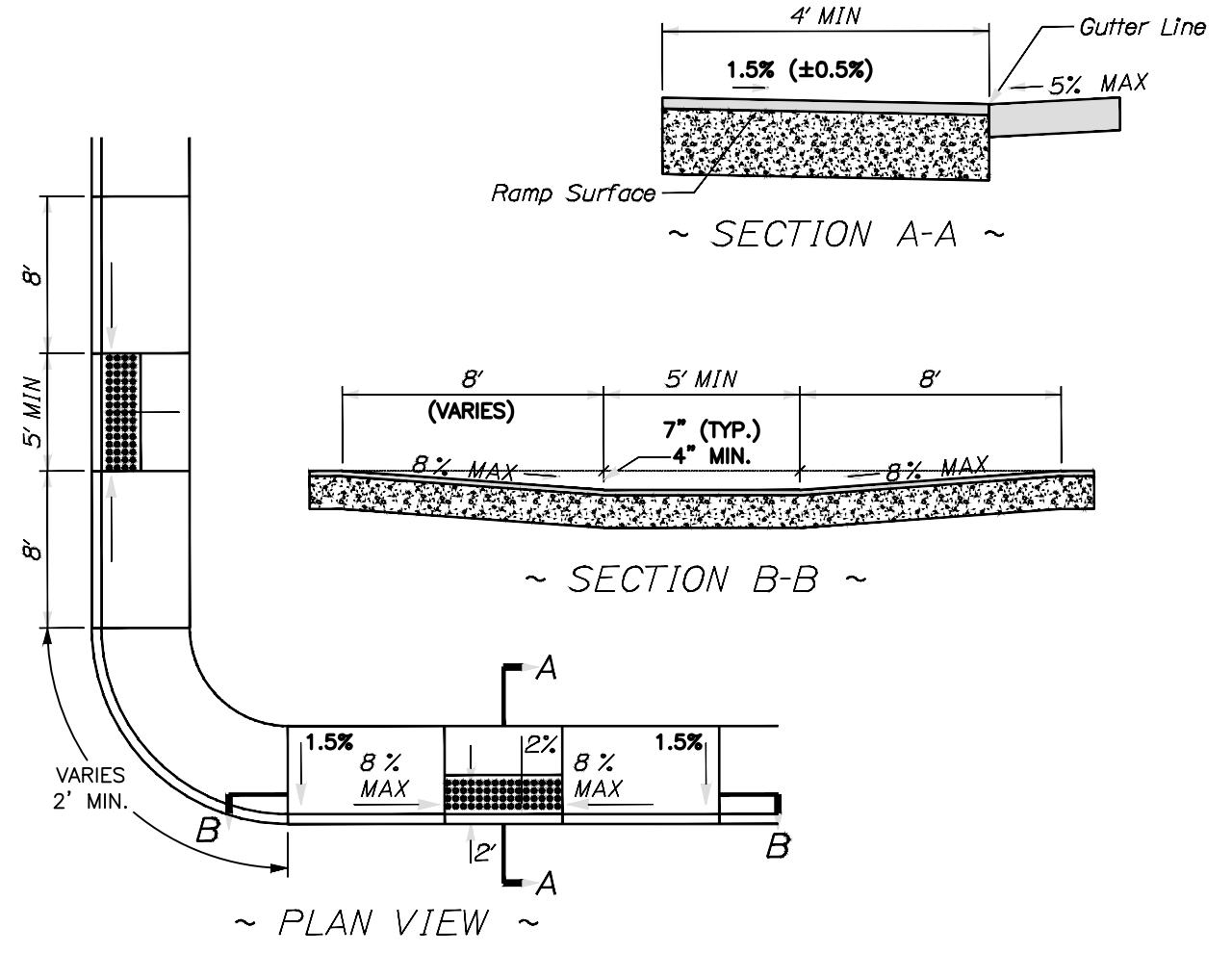


- NOTES:
1. THIS DESIGN IS THE PREFERRED OPTION WHEN THE SIDEWALK WIDTH IS GREATER THAN 6.5'. USE OTHER OPTIONS ONLY WHEN REQUIRED BY DESIGN CONSTRAINTS.
 2. A MINIMUM CURB REVEAL OF 4" IS REQUIRED AT THE APEX OF THE CURB RADIUS.
 3. MINIMUM TERMINAL CURB LENGTH SHALL BE 4'.
 4. SIDEWALK MATERIAL PER CITY SIDEWALK MATERIAL POLICY.

PERPENDICULAR PEDESTRIAN RAMP — SIDEWALK WIDTH 6.5' AND GREATER
WITH ESPLANADE
NOT TO SCALE

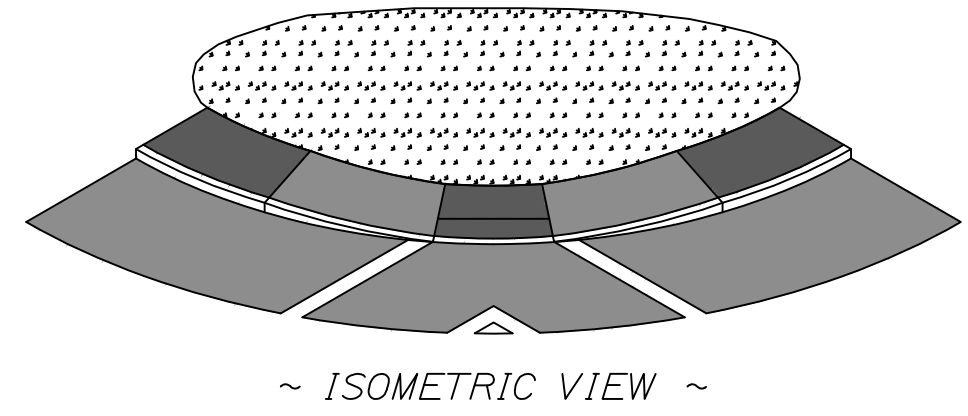


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(not to scale)

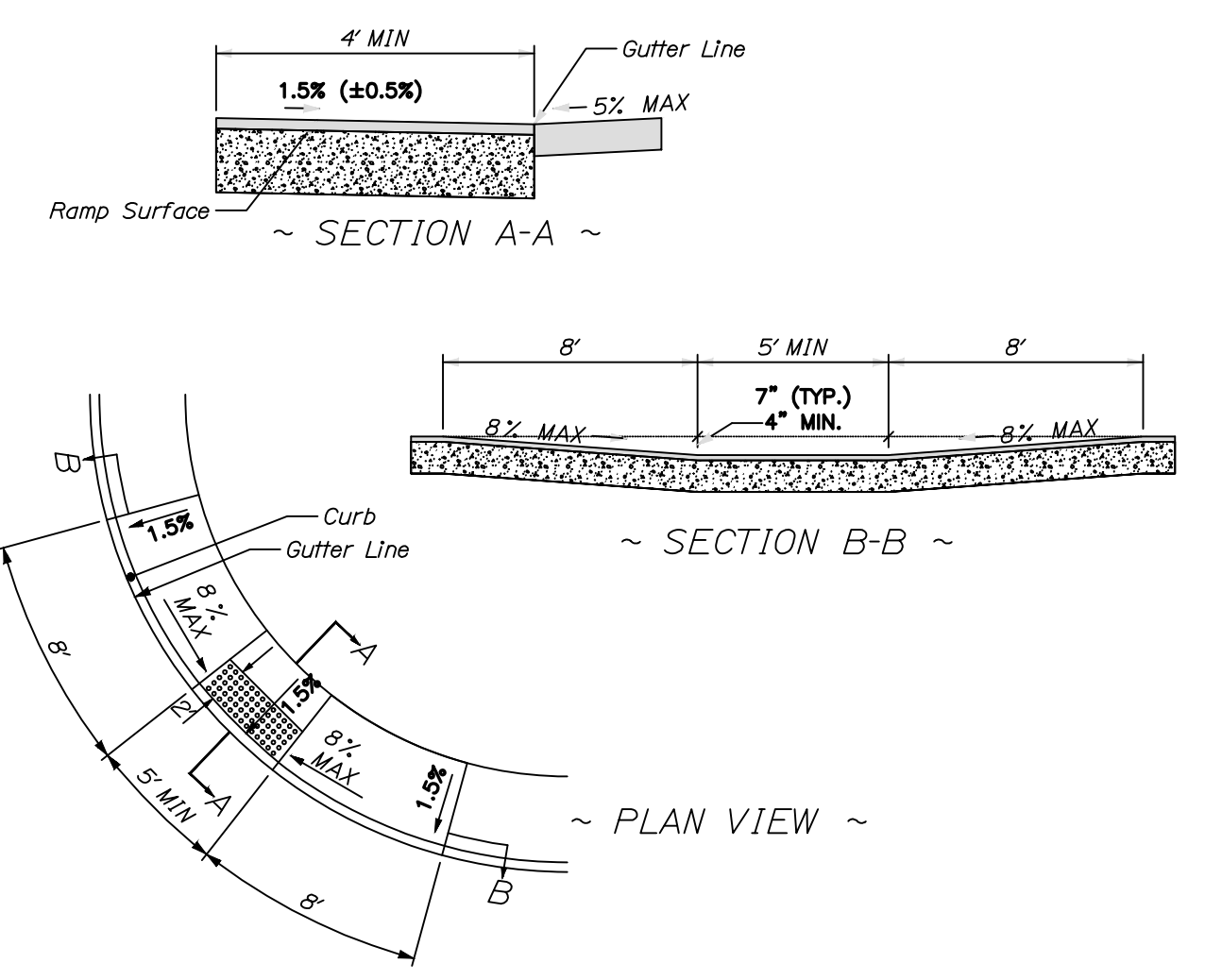


- NOTES:
1. THIS DESIGN IS THE PREFERRED OPTION WHEN THE SIDEWALK WIDTH IS LESS THAN 6.5'. USE OTHER OPTIONS ONLY WHEN REQUIRED BY DESIGN CONSTRAINTS.
 2. A MINIMUM CURB REVEAL OF 4" IS REQUIRED AT THE APEX OF THE CURB RADIUS.
 3. MINIMUM TERMINAL CURB LENGTH SHALL BE 4'.
 4. SIDEWALK MATERIAL PER CITY SIDEWALK MATERIAL POLICY.

PARALLEL PEDESTRIAN RAMP — SIDEWALKS LESS THAN 6.5' WIDE
WITH NO ESPLANADE
NOT TO SCALE

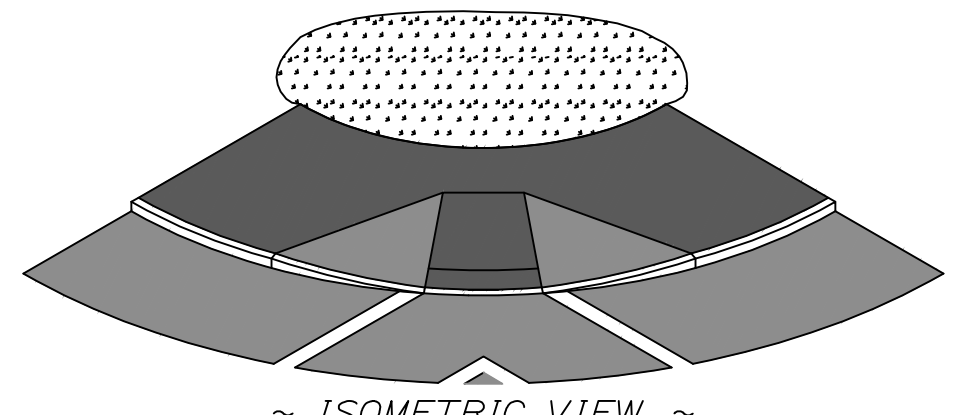


~ ISOMETRIC VIEW ~

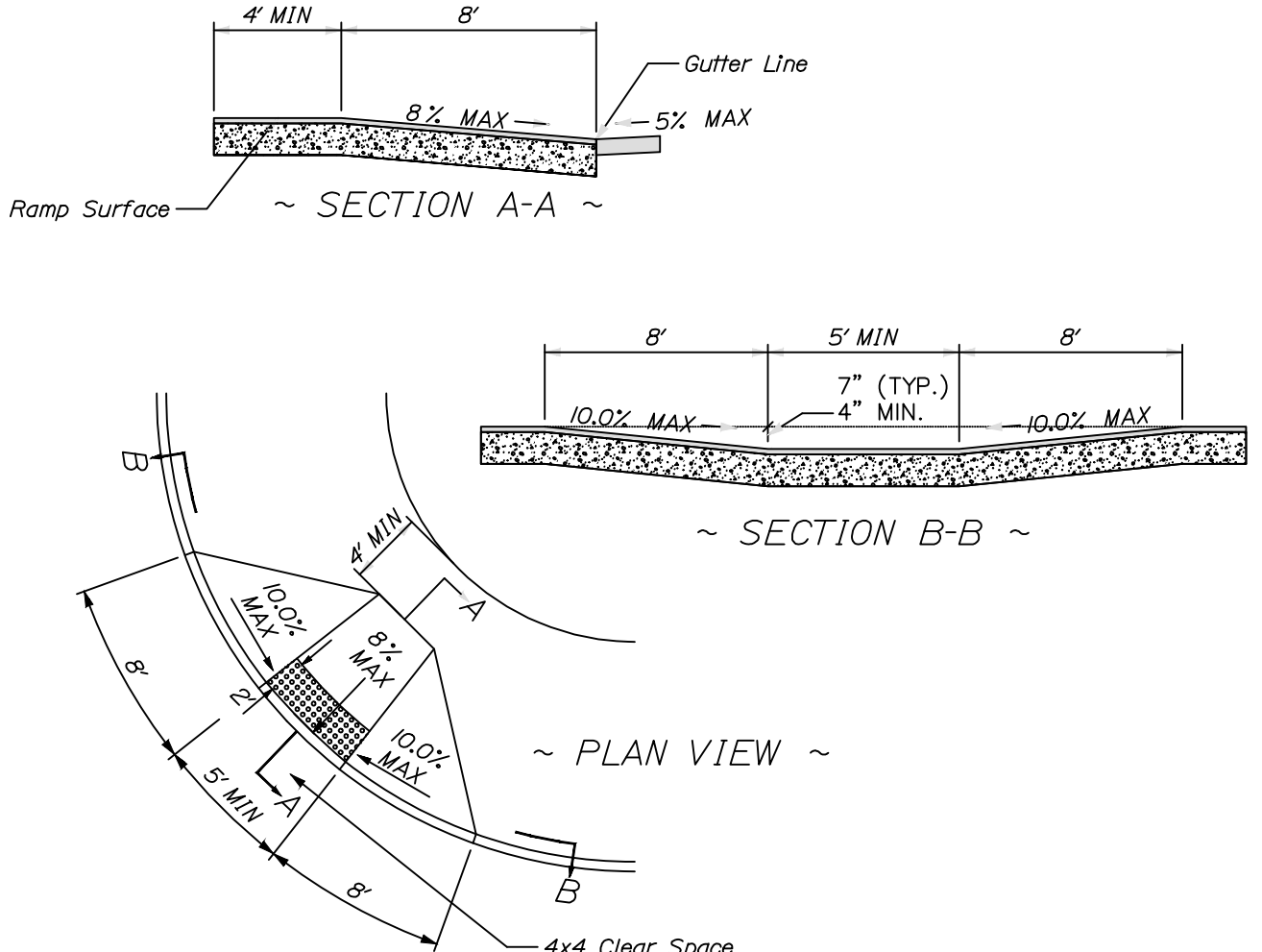


- NOTES:
1. THIS LESS DESIRABLE DESIGN SHOULD NOT BE USED UNLESS REQUIRED BY DESIGN CONSTRAINTS. IT DOES NOT PROVIDE DIRECTIONAL CUES.
 2. A MINIMUM CURB REVEAL OF 4" IS REQUIRED AT THE APEX OF THE CURB RADIUS.
 3. MINIMUM TERMINAL CURB LENGTH SHALL BE 4'.
 4. SIDEWALK MATERIAL PER CITY SIDEWALK MATERIAL POLICY.

APEX PEDESTRIAN RAMP — SIDEWALKS LESS THAN 6.5' WIDE
WITH NO ESPLANADE
NOT TO SCALE

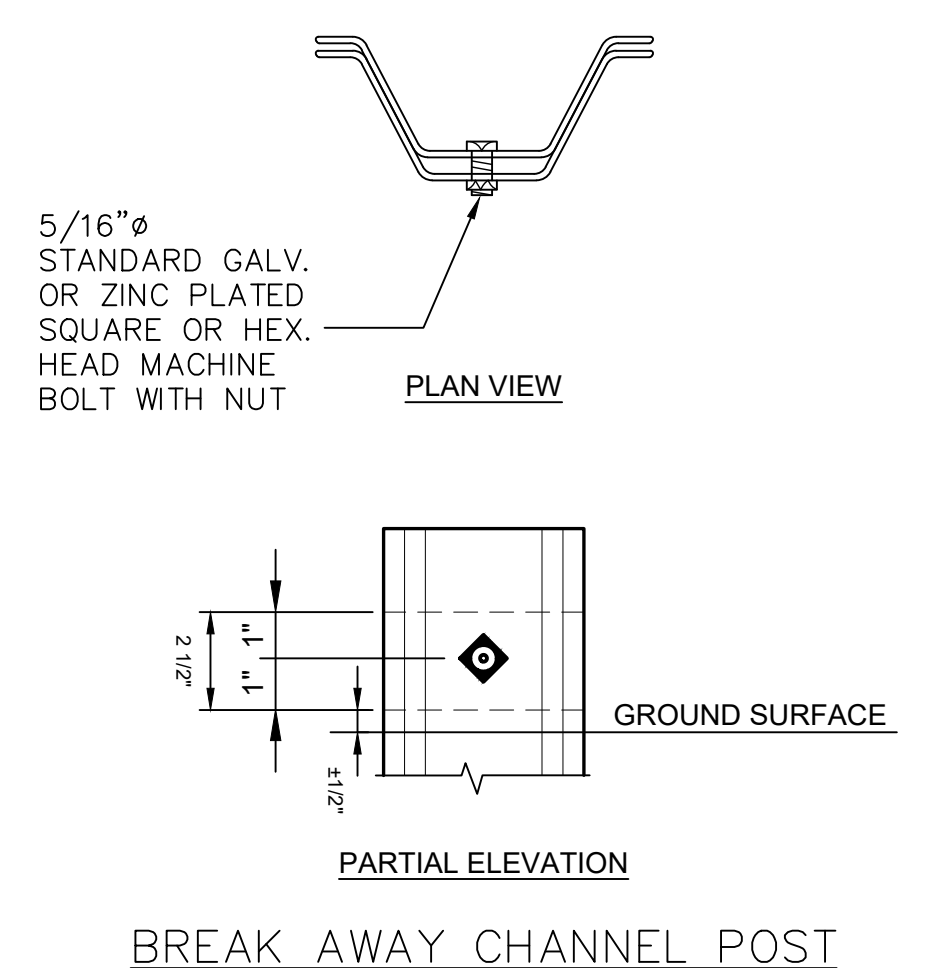
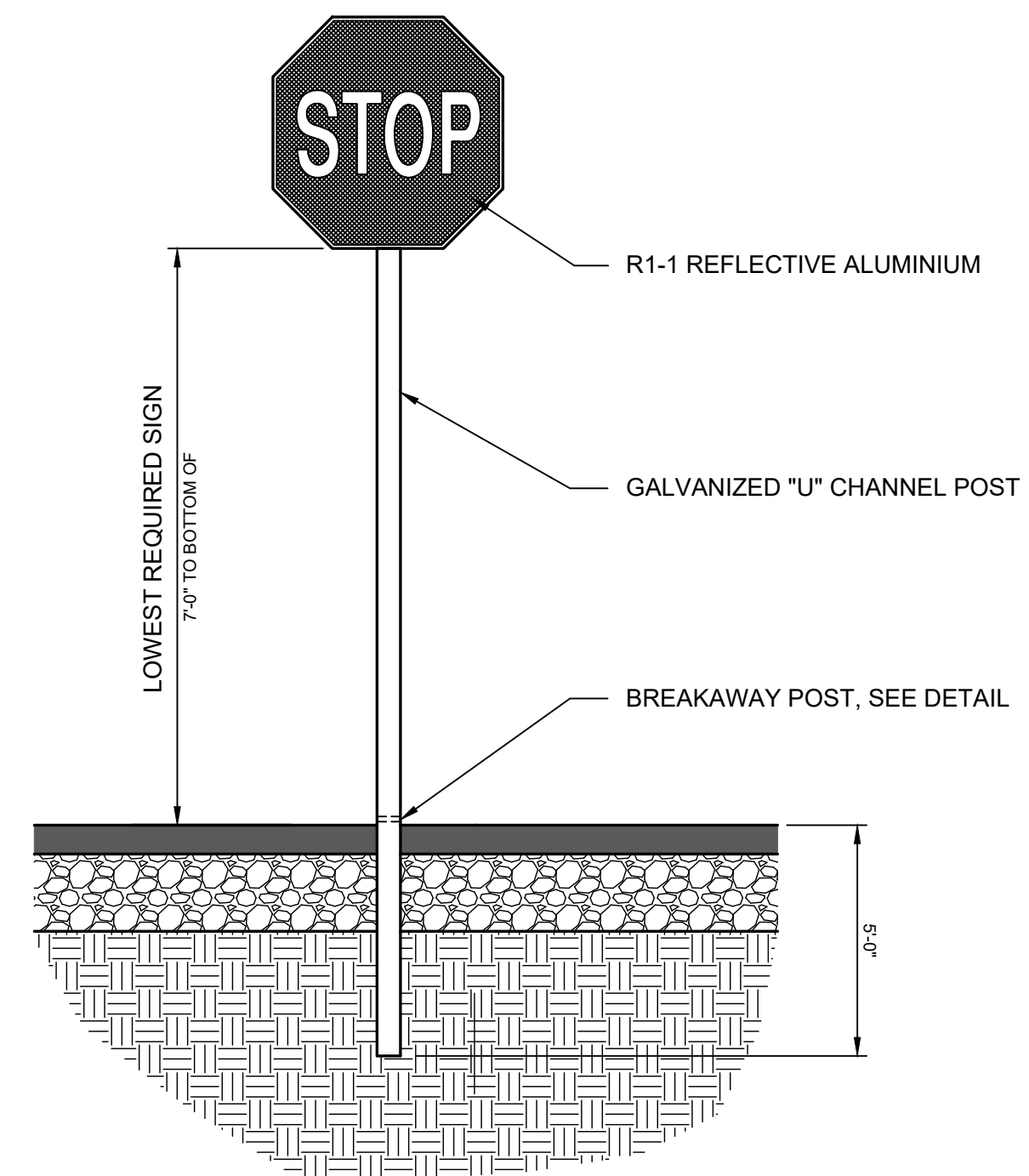


~ ISOMETRIC VIEW ~



- NOTES:
1. THIS LESS DESIRABLE DESIGN SHOULD NOT BE USED UNLESS REQUIRED BY DESIGN CONSTRAINTS. IT DOES NOT PROVIDE DIRECTIONAL CUES.
 2. A MINIMUM CURB REVEAL OF 4" IS REQUIRED AT THE APEX OF THE CURB RADIUS.
 3. MINIMUM TERMINAL CURB LENGTH SHALL BE 4'.
 4. SIDEWALK MATERIAL PER CITY SIDEWALK MATERIAL POLICY.

APEX PEDESTRIAN RAMP — SIDEWALK WIDTH 6.5' AND GREATER
WITH NO ESPLANADE
NOT TO SCALE



I:\SHARED DRIVES\DPH - PROJECTS\PROJECTS\2024\07 SIDEWALK PROJECTS\DESIGN\2024 CDBG SWV DETAILS.DWG 22 2/13/2024

DESIGN BY:	E. BOLT	DATE:	SEPT 2024
DRAWN BY:	E. BOLT	SHEET NO.:	22
CHECKED BY:	L.K. SOEZA	PROJECT NAME:	2025 CDBG SIDEWALK AND ADA IMPROVEMENT PROJECT
SCALE:	AS NOTED	CITY OF PORTLAND, MAINE	DEPARTMENT OF PUBLIC WORKS
DATE:	FEB. 28, 2019	ENGINEERING DIVISION	212 CANCO RD. SUITE 8, PORTLAND, MAINE 04101
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